

Date of issue: Tuesday, 21 November 2023

<b>MEETING</b>	<b>PLANNING COMMITTEE</b> (Councillors Iftakhar (Chair), Stedmond (Vice-Chair), Carter, Gahir, Khawar, Mann, Naveed and Satti)
<b>DATE AND TIME:</b>	WEDNESDAY, 29TH NOVEMBER, 2023 AT 6.30 PM
<b>VENUE:</b>	COUNCIL CHAMBER - OBSERVATORY HOUSE, 25 WINDSOR ROAD, SL1 2EL
<b>DEMOCRATIC SERVICES OFFICER: (for all enquiries)</b>	NICHOLAS PONTONE 07749 709868

NOTICE OF MEETING

You are requested to attend the above Meeting at the time and date indicated to deal with the business set out in the following agenda.



**STEPHEN BROWN**  
Chief Executive

**AGENDA**

**PART 1**

<u>AGENDA</u> <u>ITEM</u>	<u>REPORT TITLE</u>	<u>PAGE</u>	<u>WARD</u>
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**APOLOGIES FOR ABSENCE**

**CONSTITUTIONAL MATTERS**

- |    |                          |   |   |
|----|--------------------------|---|---|
| 1. | Declarations of Interest | - | - |
|----|--------------------------|---|---|

*All Members who believe they have a Disclosable Pecuniary or other Interest in any matter to be considered at the meeting must declare that interest and, having regard to the circumstances described in Section 9 and Appendix B of the Councillors' Code of Conduct, leave the meeting while the matter is discussed.*



<b><u>AGENDA ITEM</u></b>	<b><u>REPORT TITLE</u></b>	<b><u>PAGE</u></b>	<b><u>WARD</u></b>
2.	Guidance on Predetermination/Predisposition - To Note	1 - 2	-
3.	Minutes of the Last Meeting held on 27th September 2023	3 - 6	-
4.	Human Rights Act Statement - To Note	7 - 8	-
<b>PLANNING APPLICATIONS</b>			
5.	P/10697/015 - Waste Facility, Poyle New Cottages, Bath Road, Poyle, Colnbrook, SL3 0NT  <i>Officer's Recommendation: Delegate to the Planning Manager for Approval</i>	9 - 52	Colnbrook & Poyle
6.	P/00072/128 - Akzonobel Decorative Paints, Wexham Road, Slough, SL2 5DB  <i>Officer's Recommendation: Delegate to the Planning Manager for Approval</i>	53 - 82	Upton Lea
7.	P/00740/008 - ICI Dulux Decorator Centre, Petersfield Avenue, Slough, Berkshire, SL2 5EA  <i>Officer's Recommendation: Delegate to the Planning Manager for Approval</i>	83 - 130	Slough Central
<b>MISCELLANEOUS REPORTS</b>			
8.	Slough Local Plan Annual Monitoring Report 2022/2023	131 - 202	All
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10.	Members' Attendance Record 2023-24	211 - 212	All
11.	Date of Next Meeting - 20th December 2023	-	-

Press and Public

**Attendance and accessibility:** You are welcome to attend this meeting which is open to the press and public, as an observer. You will however be asked to leave before any items in the Part II agenda are considered. For those hard of hearing an Induction Loop System is available in the Council Chamber.

**Webcasting and recording:** The public part of the meeting will be filmed by the Council for live and/or subsequent broadcast on the Council's website. The footage will remain on our website for 12 months. A copy of the recording will also be retained in accordance with the Council's data retention policy. By entering the meeting room and using the public seating area, you are consenting to being filmed and to the possible use of those images and sound recordings.

In addition, the law allows members of the public to take photographs, film, audio-record or tweet the proceedings at public meetings. Anyone proposing to do so is requested to advise the Democratic Services Officer before the start of the meeting. Filming or recording must be overt and persons filming should not move around the meeting room whilst filming nor should they obstruct proceedings or the public from viewing the meeting. The use of flash photography, additional lighting or any non hand held devices, including tripods, will not be allowed unless this has been discussed with the Democratic Services Officer.

**Emergency procedures:** The fire alarm is a continuous siren. If the alarm sounds Immediately vacate the premises by the nearest available exit at either the front or rear of the Chamber and proceed to the assembly point: The pavement of the service road outside of Westminster House, 31 Windsor Road.

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## **PREDETERMINATION/PREDISPOSITION - GUIDANCE**

The Council often has to make controversial decisions that affect people adversely and this can place individual members in a difficult position. They are expected to represent the interests of their constituents and political party and have strong views but it is also a well established legal principle that members who make these decisions must not be biased nor must they have pre-determined the outcome of the decision. This is especially so in “quasi judicial” decisions in planning and licensing committees. This Note seeks to provide guidance on what is legally permissible and when members may participate in decisions. It should be read alongside the Code of Conduct.

### Predisposition

Predisposition is lawful. Members may have strong views on a proposed decision, and may have expressed those views in public, and still participate in a decision. This will include political views and manifesto commitments. The key issue is that the member ensures that their predisposition does not prevent them from consideration of all the other factors that are relevant to a decision, such as committee reports, supporting documents and the views of objectors. In other words, the member retains an “open mind”.

Section 25 of the Localism Act 2011 confirms this position by providing that a decision will not be unlawful because of an allegation of bias or pre-determination “just because” a member has done anything that would indicate what view they may take in relation to a matter relevant to a decision. However, if a member has done something more than indicate a view on a decision, this may be unlawful bias or predetermination so it is important that advice is sought where this may be the case.

### Pre-determination / Bias

Pre-determination and bias are unlawful and can make a decision unlawful. Predetermination means having a “closed mind”. In other words, a member has made his/her mind up on a decision before considering or hearing all the relevant evidence. Bias can also arise from a member’s relationships or interests, as well as their state of mind. The Code of Conduct’s requirement to declare interests and withdraw from meetings prevents most obvious forms of bias, e.g. not deciding your own planning application. However, members may also consider that a “non-pecuniary interest” under the Code also gives rise to a risk of what is called apparent bias. The legal test is: “whether the fair-minded and informed observer, having considered the facts, would conclude that there was a real possibility that the Committee was biased’. A fair minded observer takes an objective and balanced view of the situation but Members who think that they have a relationship or interest that may raise a possibility of bias, should seek advice.

This is a complex area and this note should be read as general guidance only. Members who need advice on individual decisions, should contact the Monitoring Officer.

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**Planning Committee – Meeting held on Wednesday, 27th September, 2023.**

**Present:-** Councillors Iftakhar (Chair), Carter, Gahir, Mann, Naveed and Satti

**Also present under Rule 30:-** Councillors I Ahmed and Dhillon

**Apologies for Absence:-** Councillors Stedmond and Khawar

**PART I**

**36. Declarations of Interest**

No declarations were made.

**37. Guidance on Predetermination/Predisposition - To Note**

Members confirmed that they had read and understood the guidance on predetermination and predisposition.

**38. Minutes of the Last Meeting held on 26th July 2023 and Extraordinary Meeting held on 6th September 2023**

**Resolved** – That the minutes of the meeting held on 26<sup>th</sup> July 2023 and the minutes of the extraordinary meeting held on 6<sup>th</sup> September 2023 be approved as a correct record.

**39. Human Rights Act Statement - To Note**

The Human Rights Act Statement was noted.

**40. Planning Applications**

The Amendment Sheet, which included details of alterations and amendments received since the agenda was circulated had been sent to Committee Members and published on the Council website. Members confirmed they had received and read it prior to the consideration of planning applications.

Oral representations were made to the Committee under the Public Participation Scheme prior to the applications being considered by the Committee as follows:-

Application P/00490/055 – American Golf, 175 Bath Road, Slough – One objector addressed the Committee.

**Resolved** – That the decisions taken in respect of the planning applications as set out in the minutes below, subject to the information, including conditions and informatives set out in the report of the Planning Manager and the Amendment Sheet circulated to

## Planning Committee - 27.09.23

Members prior to the meeting and subject to any further amendments and conditions agreed by the Committee.

### 41. P/00490/055 - American Golf, 175 Bath Road, Slough, SL1 4AA

Application
Demolition of existing building and the construction of a part 4, part 5 and part 6 storey apartment block comprising 19 x one bedrooms, 19 x two bedrooms and 12 x three bedrooms including basement level parking and landscaped open space.
Decision
<p>The Chair requested to specify, in reason for refusal 3, that the development was contrary to Policy 7 of the Core Strategy as well as the local plan policy T2. Officers agreed that this was reasonable and could be added to reason for refusal 3 of the recommendation.</p> <p>The Chair proposed to move to the officer's recommendation to delegate to the Planning Manager for refusal subject to the amendment stipulated to reason for refusal 3. This was seconded by Councillor Satti. All members present voted in favour of the recommendation.</p> <p>Delegated to the Planning Manager for refusal for the following reasons:</p> <ol style="list-style-type: none"><li>1. The proposed development would result in the loss of safeguarded employment land in an Existing Business Area and the applicant had failed to demonstrate that there are no viable options, the loss would be irreversible and would impact the job market. Furthermore, it had not been demonstrated how the proposal would not prejudice adjacent sites from being comprehensively redeveloped. The built form of development results in overdevelopment that would not achieve a high quality of design and would not enhance the quality of the built environment. The proposal was therefore contrary to policy EN1 of the Local Plan for Slough March 2004 and Core Policies 1, 5 and 8 of the Local Development Framework Core Strategy 2008 and the requirements of the National Planning Policy Framework 2023.</li><li>2. The proposed development would result in residential accommodation that fails to achieve appropriate levels of natural daylight and sunlight and result in windows immediately abutting the public footway, resulting in a substandard level of amenity for future occupiers of the development to their detriment. The proposal was therefore contrary to policies EN1 of the Local Plan for Slough March 2004 and Core Policy 8 of the Local Development Framework Core Strategy 2008 and the requirements of the National Planning Policy Framework 2023.</li></ol>

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3. The development failed to provide car parking in accordance with adopted Slough Borough Council standards and if permitted would lead to additional on-street car parking which would obstruct the access, turning heads, visibility splays, cause pavement parking or obstruct access by emergency vehicles which would be detrimental to the users of the highway including pedestrians. The development is contrary to Policy 7 of the Core Strategy 2006-2026 and Slough Borough Council Local Plan Policy T2 which requires that: 'New development is sustainable and located in the most accessible locations, thereby reducing the need to travel' and 'Residential development will be required to provide a level of parking appropriate to its location and which will overcome road safety problems, protect the amenities of adjoining residents'. The additional on-street parking would create a highway safety problem the proposals could also be contrary to the Paragraphs 110 and 112 of the National Planning Policy Framework 2023 which requires that: 'Safe and suitable access to the site can be achieved for all users' and requires developments to: 'Minimise the scope for conflicts between pedestrians, cyclists, and vehicles'.
4. The proposal would, if acceptable in other respects, be required to legally secure affordable housing units, provide for necessary infrastructure by way of appropriate financial contributions, and to secure a late stage financial viability review in respect to on-site and / or off-site affordable housing contributions, all of which would need to be secured by the completion of a section 106 agreement. No such agreement has been completed, contrary to Policies 4, 9 and 10 of the Slough Local Development Framework Core Strategy 2006 - 2026, Slough Borough Council's Developers Guide Part 2 Developer Contributions and Affordable Housing (Section 106) and to the requirements of Regulation 61 of The Conservation of Habitats and Species (Amendment) (EU Exit) Regulations 2019.

### 42. Planning Appeal Decisions July (deferred)

Members received and noted details of planning appeals determined in July.

Members raised a question on overturned appeals and whether more information could be shared on these. The Planning Manager offered to email a note to members separately following the meeting regarding this.

**Resolved** – That details on planning appeals be noted.

### 43. Planning Appeal Decisions August

Members received and noted details of planning appeals determined in August and September.

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**Resolved** – That details on planning appeals be noted.

**44. Members Attendance Record**

**Resolved** – That the record of members' attendance for 2022/23 be noted.

**45. Date of Next Meeting - 25th October 2023**

The date of the next meeting was confirmed as 25<sup>th</sup> October 2023.

Chair

(Note: The Meeting opened at 6.30 pm and closed at 7.10 pm)

The Human Rights Act 1998 was brought into force in this country on 2<sup>nd</sup> October 2000, and it will now, subject to certain expectations, be directly unlawful for a public authority to act in a way which is incompatible with a Convention Right. In particular Article 8 (Respect for Private and Family Life) and Article 1 of Protocol 1 (Peaceful Enjoyment of Property) apply to planning decisions. When a planning decision is to be made, however, there is further provision that a public authority must take into account the public interest. In the vast majority of cases existing planning law has for many years demanded a balancing exercise between private rights and public interest, and therefore much of this authority's decision making will continue to take into account this balance.

The Human Rights Act 1998 will not be referred to in the Officers Report for individual applications beyond this general statement, unless there are exceptional circumstances which demand more careful and sensitive consideration of Human Rights issues.

Please note the Ordnance Survey Maps for each of the planning applications are not to scale and measurements should not be taken from them. They are provided to show the location of the application sites.

CLU / CLUD	Certificate of Lawful Use / Development
GOSE	Government Office for the South East
HPSP	Head of Planning and Strategic Policy
HPPP	Head of Planning Policy & Projects
S106	Section 106 Planning Legal Agreement
SPZ	Simplified Planning Zone
TPO	Tree Preservation Order
LPA	Local Planning Authority

	<b>OLD USE CLASSES – Principal uses</b>
A1	Retail Shop
A2	Financial & Professional Services
A3	Restaurants & Cafes
A4	Drinking Establishments
A5	Hot Food Takeaways
B1 (a)	Offices
B1 (b)	Research & Development
B1 (c)	Light Industrial
B2	General Industrial
B8	Warehouse, Storage & Distribution
C1	Hotel, Guest House
C2	Residential Institutions
C2(a)	Secure Residential Institutions
C3	Dwellinghouse
C4	Houses in Multiple Occupation
D1	Non Residential Institutions
D2	Assembly & Leisure

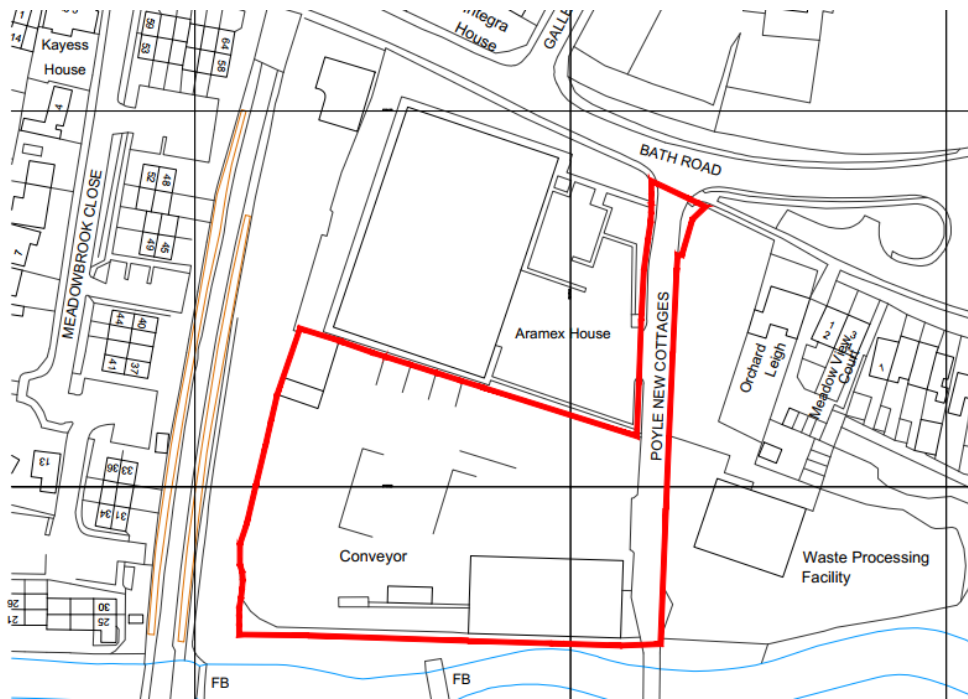
	<b>OFFICER ABBREVIATIONS</b>
DR	Daniel Ray
ADJ	Alistair de Jeux
PS	Paul Stimpson
NR	Neetal Rajput
HA	Howard Albertini
JG	James Guthrie
SB	Sharon Belcher
IK	Ismat Kausar
CM	Christian Morrone
AH	Alex Harrison
NB	Neil Button
MS	Michael Scott
SS	Shivesh Seedhar
NJ	Nyra John
KP	Komal Patel
WD	William Docherty

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Registration Date:	09-Jan-2023	Application No:	P/10697/015
Officer:	Alex Harrison	Ward:	Colnbrook & Poyle
Applicant:	DV4 Coltham (Colnbrook) Limited	Application Type:	Major
		13 Week Date:	10 April 2023
Agent:	Ben Cook, Barton Willmore, now Stantec Barton Willmore, now Stantec, Bank House, 8 Cherry Street, Birmingham, B2 5AL		
Location:	Waste Facility, Poyle New Cottages, Bath Road, Poyle, Colnbrook, SL3 0NT		
Proposal:	Redevelop the site to deliver a leading industrial and logistics unit of approximately 34,000 sqft (Mixed Use Class E(g)(iii)/B2/B8) with 6,000 sqft of ancillary office floorspace (Use Class E(g)(i)), parking, associated service yards and landscaping.		

**Recommendation:** Delegate to the Planning Manager



## **1.0 SUMMARY OF RECOMMENDATION**

1.1 Having considered the relevant policies of the Development Plan set out below, the representations received from consultees and the community along with all relevant material considerations, it is recommended the application be delegated to the Planning Manager for:

A. Approval subject to:

- (i) The satisfactory completion of a Section 106 Agreement to secure a Heavy Goods Vehicle routing and management plan, a travel plan monitoring and a financial contribution towards improvements to Colne Valley Park which are required to mitigate the impacts of development;
- (ii) Referral to the Secretary of State to consider if the application should be called in;
- (iii) Finalising conditions and any other minor changes; OR

B. Refuse the application if the completion of the Section 106 Agreement is not finalised by 1 May 2024 unless a longer period is agreed by the Planning Manager, or Chair of the Planning Committee.

1.2 The proposals comprise a major planning application; therefore the development is required to be determined by Slough Borough Council Planning Committee.

## **PART A: BACKGROUND**

### **2.0 Proposal**

2.1 Full planning permission is sought to demolish the existing buildings and structure and clear the site to redevelop it to provide a single building for employment use under Use Classes E (g)(iii)(B2/B8).

Under the Use Classes Order 1987 as amended, the proposal would be used for the following uses:

- E(g)(iii) – Industrial processes that can be carried out in a residential area without detriment to its amenity.
- B2 – General Industrial excluding processes or incineration, chemical treatment or landfill or hazardous waste.
- B8 – Storage and distribution.
- Ancillary Offices are proposed but due to their nature do not form a separate use class as they would be ancillary to the principle use.

The building will sit at the north-western part of the site and provides approximately 3,000sqm of floorspace which includes ancillary office space.

2.2 Parking for staff will be provided at the southern part of the site with 10% of spaces proposed for electrical vehicle (EV) charging and infrastructure in place for further provision in the future.

The proposal will provide:

- 39 car parking spaces
- 2 of the 39 spaces will be accessible
- 4 of the spaces will have EV charging points
- 5 HGV parking spaces

To the immediate east (front) of the proposed building is the yard area for HGVs with loading and turning space proposed. It is proposed that there would be a total of 35 HGV movements over the course of the day at the site once the site is in operation. Access to this is separate from the access to the staff parking.

2.3 Landscaping is proposed to the southern and north-western parts of the site.

2.4 The plans have been amended since their original submission to amend the roof design to incorporate a hip to the pitched roofs.

2.5 The application was originally submitted with the following technical content:

- Planning Statement
- Design and Access Statement
- Flood Risk Assessment
- Drainage Strategy
- Sustainability Statement
- Transport Statement
- Travel Plan
- Noise Assessment
- Air Quality Assessment
- Ecology Appraisal
- Archaeology Assessment
- Landscape Plan
- Landscape and Ecology Management Plan
- Arboricultural Assessment
- Tree Schedule
- Tree Constraints Plan
- BREEAM Pre-Assessment
- Landscape and Visual Impact Assessment and Green Belt Review
- Utilities and Infrastructure Review
- Building Life-Cycle Assessment
- Security Needs Assessment
- External Lighting Layout

Since first submission, the plans have been amended to accommodate comments from Officers and the following additional technical reports were received:

- Amended Plans
- Lighting Note
- Highways Note
- Daylight/Sunlight Assessment
- Revised Noise Assessment

2.6 Prior to submitting the application, the applicant engaged with the Council for pre-application discussions.

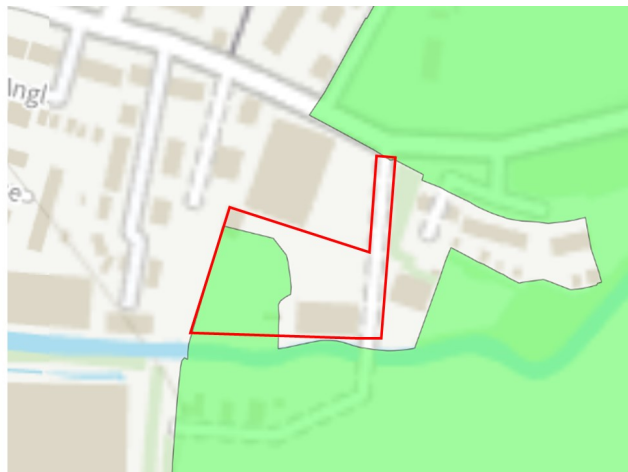
### **3.0 Application Site**

3.1 The site lies south of Galleymead House, east of the former Staines to West Drayton railway line and north of the Poyle Channel, and Poyle New Cottages. The site is just under 0.8 hectares in size.

3.2 The sites lawful use is as a recycling centre and has been used as a waste recycling centre since 1999 and is used by the operator in association with their office and vehicle workshop premises on the east side of the private access road. Over the course of the application, it was confirmed by the applicant that waste operations at the site have effectively ceased with the operator working with other waste facilities in the borough transporting waste to these sites.

3.3 Part of the site falls within land designated as Green Belt and Strategic Gap as shown in the image below. Elsewhere there are numerous open storage bunkers for recovered materials awaiting dispatch. The remaining part of the site has a weighbridge and circulation road, together with areas for skip storage.

3.4



3.5 The eastern boundary of the waste recovery site has a chain-link fence and concrete posts fronting onto the private access road. To the north a large

warehouse building is in use as well as a vacant site immediately to the west of the warehouse building with a valid planning application that has a resolution to approve for residential development subject to a Section 106 agreement, as yet undetermined (Ref: P/12934/018). To the east of the private access road and the remaining Lanz buildings, are residential properties in Bath Road.

#### **4.0 Site History**

4.1 The following accounts for the most relevant planning history at the site:

P/10697/000  
Permanent retention of waste recycling facility  
Approved 02/02/1999

P/10697/003  
Relocation of sorting hall at waste recycling centre, realignment of the Poyle Channel and associated landscape  
Approved 03/10/2002

P/10697/009  
Proposed new light line recycling facility including new building, revised access and internal layout  
Approved 22/04/2016

P/10697/010  
Demolition of existing building and installation of new light and heavy recycling facility including associated works.  
Approved 03/07/2018

4.2 Immediately to the northwest of the site there have been applications approved for development of residential units under the following reference:

P/12934/013  
Theale, Old Bath Road, Colnbrook, Slough, SL3 0NS  
Redevelopment of site to provide 27no. Flats contained within one 5 storey and one 4 storey block together with access parking and landscaping.  
(Retrospective)  
Approved 08/11/2019

To the immediate west of the site there is an application to provide 2no blocks of flats that is as yet to be determined:

P/12934/018  
Theale, Old Bath Road, Colnbrook, Slough, Slough, SL2 0NS  
Outline planning permission with some matters reserved for the redevelopment of the site to provide 24 No. flats contained within 2

separate apartment blocks. Block C will contain 4 storeys and Block D will contain 5 storeys together with access parking and hard landscaping. Under Consideration.

## **5.0 Neighbour Notification**

5.1 Due to the development being a major application, in accordance with Article 15 of The Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended), site notices were displayed outside the site on 01/02/2023 and 07/11/2023. The application was advertised in the 28/07/2023 edition of The Slough Express.

5.2 No letters have been received.

## **6.0 Consultations**

### **6.1 Highways and Transport**

#### Vehicle Access

Slough Borough Council (SBC) Highways and Transport would have no objection to the proposed development due to the proposed vehicular access arrangements for the development. The NPPF Paragraph 110 requires applications for development to provide *'safe and suitable access to the site can be achieved for all users'*.

The site is served by a private road which measures approximately 6.0m wide. The access also serves an existing industrial unit to the north of the proposed development and Longford Landfill Site and circa. 6 dwellings to the south. A sliding gate is proposed at the access to the HGV loading/servicing yard.

Drawing No. J32-6663-PS-001-Rev-A (dated 17/11/2022) has been submitted which demonstrates that visibility splays of 2.4m x 97m can be provided in each direction from the Bath Road / Site Access Junction. This visibility meets the requirements of the Design Manual for Roads and Bridges (DMRB) for the recorded 38.3pm 85<sup>th</sup> percentile speed.

The applicant has submitted Drawing No. J32-6663-PS-001-Rev-B, dated March 2023 by Mode Transport Planning which demonstrates that visibility splays of 2.0m x 43m and 2.0m x 12.9m can be provided from the proposed car park access.

The Transport Statement (TS) highlights that a single slight accident occurred at the junction of Poyle New Cottages/Bath Road in 2016, this being the most recent accident near the access. The TS outlines that the collision occurred between a cyclist and a car with the accident attributed to driver/rider error by Police.

### Pedestrian Access

An extended footway is proposed along the site access road. This provides an improved pedestrian connection to Bath Road for employees of the site and existing residents of Poyle New Cottages to the south.

### Access by Sustainable Travel Modes

The nearest bus stops to the site are 140 metres and 260 metres from the site at Galleymead Road. The number 81 Bus service provides a service between Slough Town Centre, Heathrow and Hounslow stopping every 6-17 minutes at Galleymead Road.

For Bus Stops, 400 metres walking distance is deemed acceptable by the Chartered Institute of Highways and Transport (CIHT) within their document: *'Planning for Walking and Cycling, 2015'*.

### Car Parking

The SBC Highways and Transport Officers would have no objection to the proposed number of car parking spaces for the proposed development. 39 car parking spaces are proposed on site, including 33 standard parking spaces, 2 disabled parking spaces and 4 Electric Vehicle Charging Spaces.

29 vehicles are forecast to park on site between 10:00 – 12:00 which can be accommodated within the 39 car parking spaces proposed on site. The forecast has been completed using TRICS survey data of parking demand at similar development sites.

Slough's Car Parking Standards would require the provision of 63 car parking spaces for B2 Light Industrial or 16 car parking spaces for B8 Warehousing. The parking standards are presented in the table below:

Slough Car Parking Standards (2008)			
Possible Use Types	Vehicle Type	Spaces per 100sqm	Spaces Required
B2 Industrial (3159sq.m)	Car Spaces	1 to 50m <sup>2</sup>	63
	Lorry Spaces	1 to 500m <sup>2</sup>	6
B8 Distribution and Warehousing (3159sq.m)	Car Spaces	1 to 200m <sup>2</sup>	16
	Lorry Spaces	1 to 500m <sup>2</sup> upto 2000m <sup>2</sup> then 1 to 1000m <sup>2</sup>	5

The 5 HGV parking spaces proposed are compliant with the lorry parking standard for B2 Industrial use or a shortfall of 1 lorry space for the B8 Distribution parking standard.

### Trip Generation / Traffic Impact

SBC Highways and Transport officers would have no objection to the proposed development due to forecast vehicle trip generation of the

development. The NPPF paragraph 111 states that: *‘Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe’.*

The proposed development is expected to generate 9 and 10 additional trips during the AM and PM Peak Hours, which is 1 additional trip every 6 minutes. SBC officers do not consider this number of trips likely to increase queue lengths on the surrounding road network.

### HGV Trips

Over the course of a day, the proposed development is estimated to generate 27 fewer Heavy Good Vehicles (HGV) trips every day compared to the consented Waste Processing Facility (Planning Ref: P/10697/015).

The proposed development is forecast to generate 35 two-way HGV movements over a typical day based on the TRICS trip survey data provided within the Transport Statement appendices.

This is 27 fewer HGVs than the consented 62 two-way daily HGV trips at the Waste Processing Facility based on 25,000Tonne/Annum of waste and survey data from the existing facility.

### Capacity Modelling

Junction capacity models have demonstrated that the proposed development would not have a significant impact on queueing or delay at the junctions of Bath Road/Poyle New Cottages, Bath Road / Poyle Road or Bath Road / Galleymead Road.

### Site Layout

Swept path analysis has been provided for a large car, a fire tender and a 16.5m long articulated lorry. The swept paths demonstrate there is sufficient turning space for these vehicles and that the car parking spaces can be ingress/egress by a large car measuring 5.079m in length.

### Electric Vehicle Parking

10% (4 spaces) of the car parking spaces on site should be fitted with Electric Vehicle Charging Points. The TS does not refer to EV Charging. Further details should be secured by planning condition.

The Slough Low Emissions Strategy (2018 – 2025) requires the provision of EV Charging Points for 10% of car parking spaces at new industrial or employment developments. The National Planning Policy Framework Paragraph 112 requires applications for development to: *‘Be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible, and convenient locations’.*



### Cycle Parking

SBC Highways and Transport have no objection to the proposed cycle parking. The TS states that secure and covered cycle parking is provided for 8 bicycles. 8 cycle parking spaces is compliant with the Slough Parking Standards which require 1 cycle parking space per 500m<sup>2</sup> for both B2 Light Industrial and B8 Warehousing land uses.

### Travel Plan

A Framework Workplace Travel Plan has been submitted which aims to achieve a 10% reduction in the number of people travelling by single occupancy car journey.

SBC Highways and Transport require a Section 106 Contribution of £3,000 towards Travel Plan Monitoring.

SBC request that the Travel Plan is amended to include a TRICS SAM compliant survey after full occupation of the development. This is to establish the baseline travel patterns after occupation of the development. The results of the TRICS survey should be uploaded to the TRICS database.

The applicant has stated that the amended Travel Plan will be submitted once the LHA has provided a final overall consultation response regarding the scheme.

### Summary and Conclusions

I can confirm that I have no objection to the proposed development on highways and transport grounds.

#### 6.2 Lead Local Flood Authority

Raised no objections subject to conditions.

#### 6.3 Thames Water

Thames Water would advise that with regard to foul water sewerage network infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

The application indicates that surface water will not be discharged to the public network and as such Thames Water has no objection, however approval should be sought from the Lead Local Flood Authority.

Informatives have been included.

#### 6.4 Environmental Quality

Raised no objections, subject to conditions, details noted on assessment as follows:

### Air Quality Comments

The Air Quality Assessment (AQA) has been completed by Air & Acoustic Consultants Limited and includes a construction dust assessment and operational impact assessment. The methodology followed for both is accepted.

#### *Construction*

The dust risk assessment follows the Institute of Air Quality Management (IAQM) guidance. The assessment concludes that demolition magnitude is small due to the material of the existing building; earthworks have medium risk due to the underlying soil type; the construction material is likely to be steel so considered medium risk; and there are expected to be 25 HGV movements per day therefore trackout is medium risk. Although this is contradicted later in the report by stating that the expected HGV movements is 37, it is not expected that this will significantly affect the results of the assessment.

This, in addition to low PM<sub>10</sub> concentrations nearby, indicate that human health impacts will be low during all elements of the construction phase with the exception of trackout which is medium risk, under the assumption that construction vehicles exit westbound on Bath Road as a worst case scenario.

To ensure that the assessment and recommendations are valid, it is suggested that a condition is imposed which controls the exit route to ensure it is eastbound from the site as proposed in the report. Once mitigated through the implementation of a Construction Environmental Management Plan, it is expected that dust impacts will not be significant.

#### *Operation*

In regards to the operational assessment, a detailed assessment has not been completed. Table 6.1 provides a summary of the Annual Average Daily Traffic (AADT) for Light Duty Vehicles (LDV) and HGV trips associated with the development. Travelling westbound, there are 39 LDVs and 0 HDVs, therefore the criteria for a detailed assessment is not triggered. Travelling eastbound however, there are 163 LDVs and 37 HDVs which may affect the Hillingdon Air Quality Management Area (AQMA). The report explains however that the pollutant concentrations at the two nearest continuous monitors and at Slough's closest diffusion tube monitor, are far below 90% of the AQO (36ug/m<sup>3</sup>). Sections of EPUK & IAQM (2017) guidance have been quoted which indicate that less stringent criteria can be used where there is evidence that a detailed assessment is not

necessary. Due to the low concentrations in proximity to the development, and the likelihood that the vehicles will be using the Strategic Road Network only, it is unlikely that concentrations will increase to above objective levels at sensitive receptors as a result of this development. As such, it is agreed that a detailed air quality assessment is not necessary.

An exposure assessment has not been considered however due to the proximity of the residential receptors to the proposed development and their proximity to the road, there is not expected to be an exposure risk.

Similarly, to the construction assessment, the operational assessment assumes that no HGVs will be travelling west of Poyle New Cottages. This will be secured via condition.

The operational assessment only considers vehicle movements, as the current proposals do not include plant. In the event that plant was to be included, the emissions generated by that plant would need to be assessed.

#### Environmental Noise Comments

The assessment methodology follows BS4142 for the assessment of industrial/plant noise impact. The proposed development will operate 24/7 so there is risk that the development will cause disturbance to nearby residential receptors.

To assess the impact of the development, library sound data from typical industrial activities (loading, unloading, forklifts etc) has been used. No fixed plant is included in the proposal therefore this has not been included in the assessment. In the event that plant is introduced, it is expected that the plant noise rating level would not exceed the background noise level at the nearest noise sensitive receptor, and an assessment of its impact would be required, secured via condition.

Details of movements associated with the proposed development are provided in the report.

In the updated assessment, three additional receptors have been added (R06, R07 and R08). In the original assessment, the results indicated that the rating level was lower than background noise levels at all receptors during day. During the night however, R03, R04 and R05 exceeded the background noise level. In the updated assessment, the rating level has been updated for R02, R04 and R05, remaining lower than the background noise level at all receptors during the day. During the night, only R03 and R04 exceed the background noise level by 5.4dB and 1.0dB respectively. It was not clear what assumptions had changed which resulted in R05 being 3dB below the background noise level when previously it was 1.5dB above the background noise level.

In regards to LA<sub>max</sub> levels, only the forklift has been used in the maximum noise level assessment. During the night, Table 6.2 shows that an HGV manoeuvring produces the highest noise levels, so it was not clear why the report refers to forklift noise instead. The updated report confirms that although an HGV manoeuvring has the loudest continuous sound level, the maximum sound level of the forklift is approximately 6dB(A) louder. This is therefore accepted.

In the original report, the predicted noise levels from the forklift at the receptor facades were highest at receptor R04 (64.5dB). Although this exceeds the WHO criterion, the report argued that other LA<sub>max</sub> levels are much higher so no adverse effect is expected, and mitigation will not be required. However, the time history graphs presented in Appendix B suggest that the LA<sub>max</sub> level is only exceeded at the beginning and end of the night time period, therefore there may be instances of activity during the night time period between 00:30-05:00 which will result in the WHO criterion being exceeded. As this period is the only time that the nearby receptors have respite from overhead aircraft, it is important that noise from other sources is controlled and will therefore need to be mitigated.

In the updated report, Table 6.6 shows revised predicted LA<sub>max</sub> levels at R04 and R05, and new predictions for R06, R07 and R08. The criterion level of 60dB (resulting in an internal LA<sub>max</sub> of 45dB assuming a 15dB reduction via a partially opened window) is only exceeded at R04 by 1.8dB, which the report acknowledges will need to be mitigated.

Mitigation proposals are set out in section 13.5 of the report.

## 6.5 Contaminated Land Officer

Comments provided below:

- A. *Preliminary Geo-Environmental Risk Assessment*** (Ref. no. 21-2684.01\_REP\_Colnbrook, Slough\_PRA\_2022-11-22 (Issue 2)), dated 23<sup>rd</sup> November 2022, and prepared by Delta-Simons Ltd.
- B. *Geo-Environmental Assessment*** (Ref. no. 21-2684.03), dated 23<sup>rd</sup> November 2022, and prepared by Delta-Simons Ltd.
  - Following an initial desktop assessment, potential sources of contamination were identified, and an intrusive site investigation was recommended.
  - The intrusive site investigation, monitoring and follow up assessment was carried out subsequently. As a result, it was determined that potential supplementary investigation and assessment might be needed, together with specific remediation works, in order to make the site suitable for use.

A brief outline of the necessary remedial tasks is presented and recommendations made accordingly. However, a site-specific remediation strategy is required together with a verification plan, that will then feed into a final validation report.

6.6 Archaeology

No comments received.

6.7 Neighbourhood Enforcement Team

No comments received.

6.8 London Borough of Hillingdon Council

No comments received.

6.9 Environment Agency

No comments received.

6.10 It should be noted that the site no longer falls within the Public Safety Zone (which Heathrow Airport define) and therefore no consultation is required with Heathrow Airport for this application.

**7.0 Policy Background**

7.1 Slough Local Development Plan and the National Planning Policy Framework (NPPF)

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given). The current version of the National Planning Policy Framework (NPPF) was published on 5 September 2023. Significant weight should be attached to the policies and guidance contained within the NPPF particularly where the policies and guidance within the Development Plan are out-of-date or silent on a particular matter. Relevant paragraphs of the NPPF are outlined below. However, before doing so officers first identify the relevant policies in the Development Plan which is the starting point of an assessment of the application consistent with the statutory test in section 38(6) as above. The weight to be attached to the key Development

Plan policies, and an assessment of the proposal against them, is set out within this report.

## 7.2 National Planning Policy Framework 2023:

- Chapter 2. Achieving sustainable development
- Chapter 4. Decision-making
- Chapter 6: Building a Strong Competitive Economy
- Chapter 9. Promoting sustainable transport
- Chapter 11. Making effective use of land
- Chapter 13. Protecting Green Belt Land
- Chapter 14: Meeting the challenge of climate change, flooding and coastal change

## Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document policies, December 2008:

- Core Policy 1 – Spatial Strategy
- Core Policy 2 - Green Belt and Open Space
- Core Policy 5 - Employment
- Core Policy 7 – Transport
- Core Policy 8 – Sustainability and the Environment
- Core Policy 9 – Natural and Built Environment
- Core Policy 10 – Infrastructure

## Local Plan for Slough March 2004 policies:

- CG1 – Colne Valley Park
- CG9 – Strategic Gap
- EN1 – Standard of Design
- EN3 – Landscaping Requirements
- EMP2 – Criteria for Business Developments
- EMP9 – Poyle Estate
- T2 - Parking Restraint
- T8 - Cycling Network and Facilities

## Other Relevant Documents/Statements:

- Slough Borough Council Developer's Guide Parts 1-4
- Slough Local Development Framework Proposals Map (2010)
- ProPG: Planning & Noise: Professional Practice Guidance on Planning & Noise. New Residential Development. May 2017
- Slough Low Emission Strategy 2018 – 2025

### 7.3 National Planning Practice Guidance (NPPG)

The NPPG was first published in 2014 and is iterative web-based guidance that is designed to complement the NPPF across a range of topics.

### 7.4 The Proposed Spatial Strategy (Nov 2020)

Under Regulation 18, the Proposed Spatial Strategy for the Local Plan for Slough was the subject of public consultation in November 2020. This sets out a vision and objectives along with proposals for what the pattern, scale and quality of development will be in Slough.

The consultation document contained a revised Local Plan Vision which supports the Council's vision for Slough as a place where people want to "work, rest, play and stay."

It should be noted that the consultation document for the Proposed Spatial Strategy does not contain any specific planning policies or allocate any sites. It made it clear that the existing planning policy framework for Slough would remain in force until replaced by new Local Plan policies in the future. Nevertheless, it sets out the most up to date statement of the Council's position with regards to strategic planning issues.

### 7.5 Equality Act

In addition, Section 149 of the Equality Act (2010) which sets a Public Sector Equality Duty (PSED) came into force in April 2011 and requires the Council to consider the equality impacts on all protected groups when exercising its functions. In the case of planning, equalities considerations are factored into the planning process at various stages. The first stage relates to the adoption of planning policies (national, strategic and local) and any relevant supplementary guidance. In coming to a recommendation, officers have considered the equalities impacts on protected groups in the context of the development proposals as set out below in this report.

## 8.0 **Planning Considerations**

8.1 The planning considerations for this proposal are:

- Principle of Development
- Green Belt considerations
- Impact Upon the Strategic Gap
- Impact Upon the Colne Valley Park
- Design and impact on the character and appearance of the area

- Landscape and ecology
- Impacts on neighbouring amenity
- Highways and parking
- Drainage and flood risk
- Energy and sustainability
- Air quality
- Land contamination
- Equalities considerations
- Presumption in favour of sustainable development

## **9.0 Principle of Development**

- 9.1 Paragraph 81 of the National Planning Policy Framework seeks to create conditions in which businesses can invest, expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development. Each area should be allowed to build on its strengths, counter any weaknesses and address the challenges of the future. Areas with high levels of productivity should be able to capitalise on their performance and potential.
- 9.2 Core Policy 5 (Employment) of the Core Strategy requires “*major warehousing and distribution developments be located in the eastern part of the borough and in Existing Business Areas that have good access to the strategic road and rail network*”. Local Plan saved Policy EMP2 states business development should be well designed and appropriate in scale and height, does not harm the character of the area or amenities of neighbouring land uses and can be accommodated within the existing highway network. Policy EMP9 states that proposals for B2, B8 and B1b and c (now known as E(g)(ii) and (iii)) will be permitted within the Galleymead Road and Poyle Estates.
- 9.3 Although it is not a safeguarded employment site, it is an existing business use which and the proposed development will retain the use of the site as such.
- 9.4 However, the proposed development would constitute inappropriate development within the Green Belt which would normally be considered contrary to the Development Plan for Slough Borough Council, in particular Core Strategy Policies Core Policy 1 and Core Policy 2 and Local Plan Policies CG1 and CG9. Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise.

In this instance, it is for the Applicant to demonstrate to the satisfaction of the Local Planning Authority if there are (or, if it remains that there are) Very Special Circumstances which demonstrate whether there are other



material circumstances that need to be taken into account, notwithstanding the development plan provisions. This is addressed in the following section.

- 9.5 Therefore, the principle of development will only be considered acceptable if the applicant has been able to demonstrate Very Special Circumstances for development in the Green Belt.

## **10.0 Green Belt considerations**

- 10.1 The site is located within the Green Belt and Strategic Gap. Core Policy 1 of the Core Strategy requires all development to take place within the built-up area, predominantly on previously developed land, unless there are very special circumstances that would justify the use of Green Belt. Core Policy 2 states, among other things, that opportunities to enhance the quality of the Green Belt will be taken and that development will only be permitted in the Strategic Gap and the open areas of the Colne Valley Park if it is essential to be in that location. Paragraph 149 of the National Planning Policy Framework states the construction of new buildings as inappropriate in the Green Belt. A number of exceptions are listed as follows:

- a) buildings for agriculture and forestry;
- b) the provision of appropriate facilities (in connection with the existing use of land or a change of use) for outdoor sport, outdoor recreation, cemeteries and burial grounds and allotments; as long as the facilities preserve the openness of the Green Belt and do not conflict with the purposes of including land within it;
- c) the extension or alteration of a building provided that it does not result in disproportionate additions over and above the size of the original building;
- d) the replacement of a building, provided the new building is in the same use and not materially larger than the one it replaces;
- e) limited infilling in villages;
- f) limited affordable housing for local community needs under policies set out in the development plan (including policies for rural exception sites); and
- g) limited infilling or the partial or complete redevelopment of previously developed land, whether redundant or in continuing use (excluding temporary buildings), which would:
  - not have a greater impact on the openness of the Green Belt than the existing development; or
  - not cause substantial harm to the openness of the Green Belt, where the development would re-use previously developed land and contribute to meeting an identified affordable housing need within the area of the local planning authority.

- 10.2 Paragraph 147 of the National Planning Policy Framework states inappropriate development is, by definition, harmful to the Green Belt and should not be approved except in very special circumstances.
- 10.3 Paragraph 148 of the National Planning Policy Framework requires Local Planning Authorities should ensure that substantial weight is given to any harm to the Green Belt. 'Very Special Circumstances' will not exist unless the potential harm to the Green Belt by reason of inappropriateness, and any other harm resulting from the proposal, is clearly outweighed by other considerations.
- 10.4 With this proposal, the applicant acknowledges that the development would result in a greater overall impact upon the openness of the Green Belt and would not comply with para 149 of the NPPF. While the proposal could fall into category G of para 149, the scale of works proposed are such that it is considered to have an impact on the openness of the Green Belt than the existing development does. As a result, the applicant has submitted a 'Landscape and Visual Impact Assessment and Green Belt Review' and rely on their Planning Statement to demonstrate and justify 'Very Special Circumstances'.
- 10.5 The applicant states that the following points should amount to 'Very Special Circumstances' that would enable to proposal to be considered favorably in terms of its impact on the Green Belt:

- The site does not perform against the five purposes of the Green Belt.

These purposes are set out at para 138 of the NPPF and are as follows:

- a) to check the unrestricted sprawl of large built-up areas;
  - b) to prevent neighbouring towns merging into one another;
  - c) to assist in safeguarding the countryside from encroachment;
  - d) to preserve the setting and special character of historic towns;
- and
- e) to assist in urban regeneration, by encouraging the recycling of derelict and other urban land.

- 10.6 In considering the justification put forward by the applicant, it is apparent that, while the site is partially within the Green Belt it is not a site that can be considered to be open in character as existing. Development within it is therefore unlikely to check the unrestricted sprawl of larger built-up areas. As a note to this, Green Belt land by definition is not necessarily undeveloped land. It is more typical that such land on the edge of settlements is undeveloped as it has a purpose of controlling sprawl. The site is not considered to achieve this currently and the impact would be no different through the proposal. Similarly, the application proposal would not

result in further encroachment of development into the countryside as it is already developed.

- 10.7 It is considered that there is no implication in respect of preventing neighbouring towns merging. Additionally, the role of the site at present does not play a part in preserving the setting and special character of a historic town.
- 10.8 It is possible to make a case that the proposal results in urban regeneration through recycling urban land due to its previously developed nature although the goal of this point is relevant to non-Green Belt land.
- 10.9 Given that the site is partially located within the Green Belt and is entirely developed it is agreed that the role of the site make no contribution to restricting urban sprawl and stopping encroachment into the countryside. While the development to provide a larger single building on the site will have a further impact on the openness of the Green Belt, it is considered that this impact does not result in any further harm to the Green Belt as a result. Therefore, it is considered that the proposal has Very Special Circumstance to allow the development of the site as a matter of principle, as an exception to Green Belt policy.
- 10.10 One point to note is that supporting this application proposal would not remove the site from the Green Belt. While the applicant makes the case that the site should not be within the Green Belt, it is not the place for Development Management to determine the extent of the allocation. Green Belt is added and/or removed through the Local Plan process which means this aspect is not a consideration with this application.
- 10.11 A final point is that as the application is for development within the Green Belt it is a departure from the Local Plan which resists such development. Therefore, if Members are minded to support the application it will need to be referred to the Secretary of State to determine whether or he or she should consider and determine the application.
- 10.12 **Impact Upon the Strategic Gap**
- 10.13 Local Plan policy CG9 states that any development within the Strategic Gap will not be approved where it threatens the clear separation or the role of open land between Slough and Greater London.
- 10.14 The difference between the lawful use of this site is as a waste transfer site and the proposed employment use would not change or result in the loss of any separation between Slough and Greater London or loss of open land

given that the entire site is used for commercial purposes because the site is previously developed land and currently makes no contribution to the purposes of the gap itself. The proposal would not therefore have a detrimental impact on the Strategic Gap.

#### **10.15 Impact Upon the Colne Valley Park**

10.16 Local Plan policy CG1 seeks to control development in the Colne Valley Park and where development is permitted to ensure that appropriate mitigation measures are undertaken to realise the aims and objectives of the Colne Valley Park.

10.17 The site is not part of the open area of the Colne Valley Park and there are no existing leisure uses associated with it. Its redevelopment does not therefore result in any loss of this principal activity in the Colne Valley Park.

10.18 The previous approval to redevelopment site (P/10697/010) required a contribution towards improvements to Colne Valley Park and a financial contribution is also considered appropriate for the current proposal as well. The amount is to be agreed with the applicant and an update will be provided on the Amendment Sheet.

#### **11.0 Design and Impact on Appearance and Character of the area**

11.1 The National Planning Policy Framework encourages new buildings to be of a high-quality design that should be compatible with their site and surroundings. Policy EN1 of the Local Plan outlines that development proposals are required to reflect a high standard of design and must be compatible with and/or improve their surroundings in terms of scale, height, massing, layout, siting, building form and design, architectural style, materials, access points, visual impact, relationship to nearby properties, relationship to mature trees, and relationship to water course. Poor designs which are not in keeping with their surroundings and schemes that overdevelop the site will not be permitted.

11.2 Core Strategy Policy 8 states that all development in the borough shall be sustainable, of a high-quality design, improve the quality of the environment and address the impact of climate change. Core Policy 8 outlines:

*‘All development will:*

- a) Be of a high quality design that is practical, attractive, safe, accessible and adaptable;*
- b) Respect its location and surroundings;*
- c) Provide appropriate public space, amenity space and landscaping as an integral part of the design; and*
- d) Be in accordance with the Spatial Strategy in terms of its height, scale, massing and architectural style.’*

- 11.3 The proposed development will introduce a higher element of building into the streetscene compared to the existing site. Permission was previously granted for a building of comparable scale under ref: P/10697/010. This is a material consideration that is given reduced weight as that permission has since expired without being implemented. The building will be comparable to the scale of the building to the immediate north, albeit slightly taller. The applicant has provided sections to show the proposal in the context of its neighbours and while it will be larger it is considered that the relationship would not be so overly dominant that it would result in harm to the character of the area. The relationship with residential properties is discussed in Section 13.
- 11.4 The proposal significantly increases the bulk of built form on the site. The existing site has notably smaller buildings and structures and has the character of an open yard. The proposal would make the building the prominent aspect of the site. The footprint and location of the site are comparable to the previously approved scheme and the bulk is comparable to the existing unit to the north of the site.
- 11.5 The impact of the height and bulk of the building (as a matter of principle) should be balanced against the NPPF guidance on economic growth and paragraph 80 referred to in para 9.2 of this report. The increased height and bulk allows for greater flexibility in operational terms and provides a flexible industrial space for future occupiers as it allows for greater space for equipment such as racking. This is characteristic of a lot of modern employment building proposals and shows designs responding to modern business operations at this scale. In this instance the increased height and bulk is considered to be acceptable.
- 11.6 External lighting is proposed, and details have been included that show that lighting will appear to be considerate to neighbours and not result in an overly lit site during the night hours. The Neighbourhood Enforcement Team has been consulted and no comments have been received to date, Members will be updated on the Amendment Sheet. Boundary treatment details have been provided to show 2.4m high paladin fencing to the north, east and west boundaries, these are considered to be appropriate as they reflect the existing boundary treatment and the one in place at the site opposite. A 2m high acoustic fence is proposed between the yard and car park which would also act to buffer sound from the yard to the south. Details have been secured as part of the approved plans condition.
- 11.7 The site lies within the Colne Valley Regional Park and is within the Strategic Gap. Core Policy 2 states that development will only be permitted in these areas if it is essential to be in that location. In this instance the site is already developed with a lot of activities being in the open. The proposed development will increase the bulk of development on the site and would impact the openness as a result however, in similar circumstances to the previous Green Belt considerations, the site is not part of the open areas of the Colne Valley Regional Park and makes no contribution to its value.

Similarly, its developed nature of the site means it does not contribute to the role of the Strategic Gap and the increase in bulk on site will not have an adverse impact on the Strategic Gap in this location.

11.8 Based on the above, the proposal would have an acceptable impact on the character and visual amenity of the area and therefore comply with Policies EN1, EN3, CG1, CG9 and EMP2 of the Local Plan for Slough March 2004, Core Policies 2 and 8 of The Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document, and the requirements of the National Planning Policy Framework 2023.

## 12.0 **Landscaping and ecology**

12.1 The proposal includes soft landscaping to the southern part of the site as it adjoins the watercourse. The landscaping will provide a softer edge to the southern boundary. Some landscaping is also proposed at the north-western corner of the site to provide a partial screen to at this part.

12.2 Detailed landscape proposals have been submitted with the application that show a mix of trees, hedging and shrubs proposed at the site.

12.3 The existing site is devoid of landscaping features and therefore the proposals represent an improvement to the landscaping character at the site. The gains are particularly minimal, but they are considered to be acceptable in principle. The landscaping is acceptable with regards to species, layout, planting densities and management, however it is considered that more can be done at the north-western corner to screen the building and therefore it is proposed that a condition is included to require a revised landscaping scheme for this area that results in improved screening from the west.

12.4 The submitted application included an Arboricultural Impact Assessment which sets out how the development can take place without harming the vitality of the existing trees to the south of the site. This can be secured by condition.

12.5 In terms of Ecology, no habitats or species of note are identified. The Ecological Appraisal undertaken by 'fcpr' recommends demolition takes place outside of nesting season to ensure no disturbance to birds. The assessment recommends native species to be included in the proposed landscaping to give opportunities for the creation of new habitats from the proposal. To ensure that the development provides a net gain in biodiversity it is required that the development includes the provision of bird boxes at the site, and this can be secured by condition.

## 13.0 **Impact on neighbouring amenity**

13.1 The National Planning Policy Framework encourages new developments to be of a high-quality design that should provide a high quality of amenity for

all existing and future occupiers of land and buildings. This is reflected in Core Policy 8 of the Core Strategy and Local Plan Policy EN1.

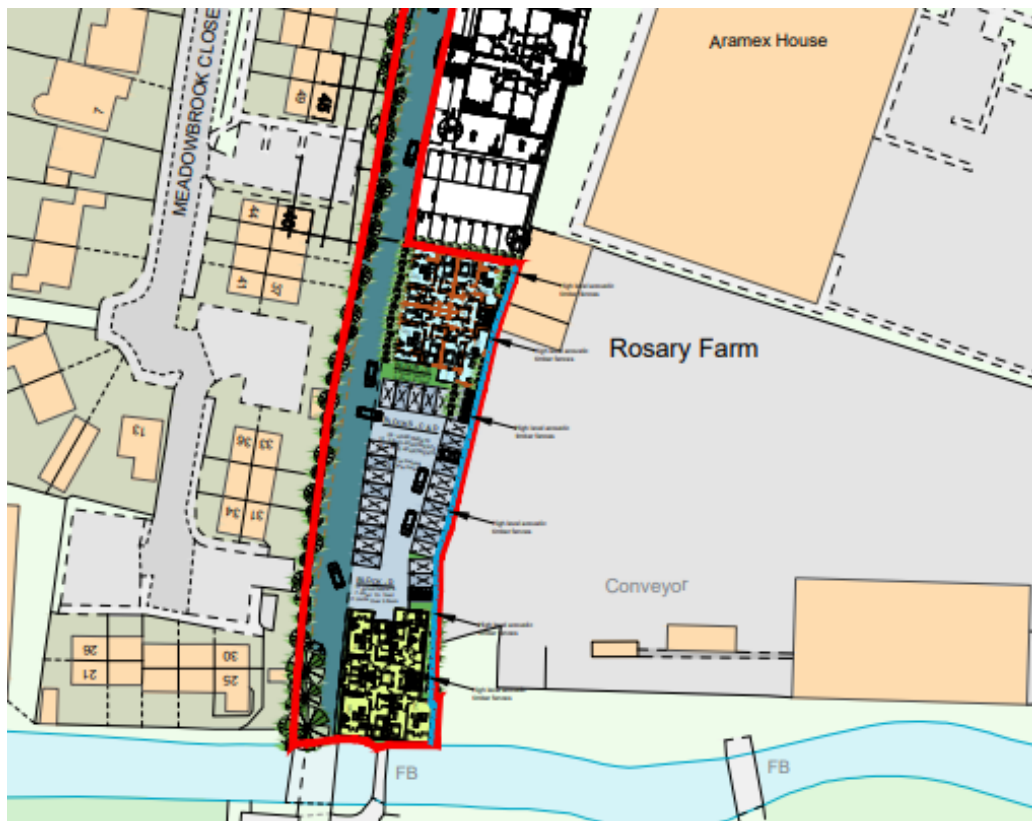
- 13.2 The site has a relationship with existing residential properties to the west and south of the site although those to the south are separated by the existing route of the brook and landscaping. There are also newly built flats to the northwest of the site and a current proposal for flats to the immediate west of the site.
- 13.3 The current site operations involve recycling materials in an outdoor location. There is therefore an existing impact of industrial operations from the site as a baseline scenario. There are also noise impacts from aircraft in this area given its proximity to Heathrow Airport.
- 13.4 In terms of noise impacts the Environmental Quality (EQ) Officer has reviewed the initial and revised Noise Assessments and has raised no objections. Officers have required the applicant to provide significant information on noise impacts to existing, forthcoming and potential residential units close to the site. The EQ Officer is satisfied that, subject to mitigation, the impacts will not amount to a significant adverse impact on amenity through noise impact.
- 13.5 In terms of mitigation the EQ Officer has requested the following:
- Construction noise: A Construction Environmental Management Plan must be submitted which contains details of noise reduction methods to comply with a noise limit of 75dB(A).
  - Plant noise: In the event that plant is introduced, it is expected that the plant noise rating level would not exceed the background noise level at the nearest noise sensitive receptor, and an assessment of its impact would be required.
  - HGV cap: Throughout the occupation of the development hereby permitted, non-electric HGV (greater than or equal to 25 tonnes) movements shall be restricted to an average of 12 individual movements (arrivals and departures) in any hour during daytime hours between 07:00 and 23:00, and an average of 4 movements in any hour during night time hours between 23:00 and 07:00.
  - Breakout noise monitoring: Throughout the occupation of the development, internal noise levels shall be less than the specified levels in the AAC Noise Assessment.

These mitigation requirements can be secured by condition and have been included as part of the recommendation.

- 13.6 In terms of impacts on daylight and sunlight, it is considered that there are suitable separation distances from existing properties to the west (approx 35m) and to the south (over 50m) that would ensure there would be no adverse impact on natural daylight and sunlight to these properties and no significant adverse harm through overshadowing. To the northeast, the

newly constructed flats were not considered to have any adverse impact that would be over and above that experienced from the existing relationship of these units to the existing commercial building to the immediate north of the application site. For the same reasons the proposed development is also not considered to be overbearing to any of these properties either.

- 13.7 Consideration has to fall to the potential impacts on units proposed in an, as yet undetermined, application to the immediate west of the site. Under application ref: P/12934/018, two apartment blocks are proposed on the land to the immediate west of the site. The scheme is for outline planning permission with detailed designs reserved and therefore not formally proposed. The indicative relationship of the proposed buildings to this application proposal are shown below:



Extract plan taken from P/12934/018

- 13.8 There are two important matters to note in relation to the adjacent development proposed under P/12934/018. Firstly, this is a proposed scheme and while a formal resolution to grant planning consent was approved at committee on 23<sup>rd</sup> April 2022, the decision has not been issued and therefore, while the application is a material consideration, and the impacts are required to be considered the status is such that there is no extant consent on the neighbouring site. Had consent been issued, it would be incumbent on the applicants for the residential development to successfully design a detailed scheme where the relationship between sites is acceptable in planning terms (as design was excluded from the Outline consent), and this would be considered at reserved matters stage.



13.9 Notwithstanding this, officers have considered the potential impact based on the indicative scheme presented and make the following observations:

- a) The southernmost block would be close to the southwestern corner of the site which will be where the car park area is located. There would not be an immediate prominence of the bulk of the proposed building in relation to this proposed apartment although there would be a visual impact. The impact itself is considered to be minor and it would not be overbearing to potential future residents and it would not adversely affect natural light to the proposed windows. The northernmost block would have a close relationship to the application building and there would be little separation distance between the two, approximately 5 metres. The proposed indicative plans for P/12934/018 show that the proposed apartment block will have windows serving habitable rooms that would face onto the application site and would therefore be affected by the proposed building. However, it would be for the detailed design reserved matters application to demonstrate an acceptable relationship.
- b) The northernmost block would have a close relationship to the application building and there would be little separation distance between the two. The proposed plans for P/12934/018 show that the proposed apartment block will have windows serving habitable rooms that would face onto the application site and would therefore be affected by the proposed building. The applicant has submitted a Daylight and Sunlight Assessment to consider the impact on this proposed apartment block. The assessment concludes that the majority of the windows (8 of 12) that face onto the site will be adversely affected by the application proposal. The assessment recommends a number of amendments to the proposed apartment scheme to improve this situation. The proposed plans have amended the roof to a hipped structure to reduce the extent of bulk from the application proposal. Again, these matters are for the residential development to submit the appropriate design at reserved matters stage.

13.10 Additionally, while the resolution to grant planning permission was approved at committee in April 2022, alterations to the proposal have been discussed with the applicant including matters relating to the relationship with the site the subject of this application and amendments are to be proposed with the application due to return to Planning Committee for members to consider at a future date.

13.11 Members are required to consider the application on the basis of the planning policy framework and material considerations that are apparent at that time. In this instance it is considered that the scheme does not, at this

time, have a demonstrable adverse impact on the potential development to the west of the site.

- 13.12 As a result of the above assessment, there is no demonstrable adverse impact on neighbouring amenity and the proposal is considered to be acceptable in light of Core Policy 8 of the Local Development Framework Core Strategy and Policies EN1 and EN2 of the Adopted Local Plan 2004.

#### **14.0 Highways and Parking**

- 14.1 The National Planning Policy Framework states that planning should seek to development is located where the need to travel will be minimised and the use of sustainable transport modes can be maximised. Development should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and pedestrians. Where appropriate local parking standards should be applied to secure appropriate levels of parking. This is reflected in Core Policy 7 and Local Plan policies T2 and T8. Paragraph 111 of the National Planning Policy Framework states that *'Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe'*.
- 14.2 The proposed access arrangements utilise the existing road and there are no objections to this. The road itself is a private road and serves two other units as well as the 6 dwellings to the south. The entrance into the site is acceptable and the submission shows that HGVs can enter, turn within the site and leave in a safe and convenient manner. A separate entrance for staff vehicles is proposed which separates these cars from the HGV movements.
- 14.3 An extended pedestrian access is proposed along the site access which improves accessibility to the site.
- 14.4 The proposal provides 39 car parking spaces at the site which would include 2 accessible spaces. 4 of the remaining 37 spaces would be fitted with EV charging points, amounting to a policy compliant 10%. The level of parking is considered to be acceptable in planning terms. In the yard area, 5 HGV parking spaces are proposed, which is also considered to be acceptable.
- 14.5 In terms of vehicle trips the proposed development will result in a reduction of vehicle movements by 27 HGV trips compared to the consented limit of the current use. There would be an increase in car trips as the current use makes no provision for car parking.
- 14.6 The application includes a Travel Plan which achieves a 10% reduction in the number of people travelling by single occupancy car journey. In accordance with the Developer's Guide a contribution is required to enable the Council to monitor this plan, in this instance the identified fee is £3,000.

14.7 Conditions are required to ensure the development is implemented in an acceptable manner. Conditions proposed relate to access, visibility splays, layout and parking, EV parking and construction management plan. A condition is also proposed to require the submission of an amended Travel Plan to include a TRICS compliant survey after the full occupation of the site to establish the baseline travel patterns that will enable the monitoring to be effective.

14.8 Subject to conditions, the scheme is not considered to have an adverse impact on highway safety and convenience and the scheme is therefore considered to be acceptable in light of Core Policy 7 of the Core Strategy, Policy T2 of The Adopted Local Plan for Slough 2004 and the parking standards set out in Developer Guide 3 (Parking Standards Table 5) and the NPPF.

### **15.0 Drainage and flood risk**

15.1 The site is located within flood zone 1 and therefore flood risk is minimal. Detailed drainage information was submitted with the application and considered by consultees, conditions have been recommended.

15.2 Thames Water have reviewed the application and have raised no objection and the Local Lead Flood Authority similarly has no objections subject to conditions, all of which are included in the recommendation.

15.3 There are no objections in respect of drainage and flood risk as a result.

### **16.0 Energy and sustainability**

16.1 Core Strategy Policy Core Policy 8 (Sustainability and The Environment) seeks development proposals to be sustainable, of a high-quality design, improve the quality of the environment and address the impact of climate change.

16.2 The application was accompanied by a BREEAM pre-assessment and an Energy Strategy. The statements advise that the proposal will meet a BREEAM rating of 'Excellent' which exceeds the requirement of Core Policy 8.

16.3 The Sustainability Statement confirms that the development adopt a fabric-first approach to improve thermal performance, and utilise air source heat pumps for heating and cooling the office areas.

16.4 The implementation of the identified measures and achieving a BREEAM Excellent rating would ensure the development is acceptable in light of Core Policy 8. It will be necessary to include a condition that requires the applicant to prove that the BREEAM rating has been achieved.

## 17.0 **Air Quality**

- 17.1 Core Policy 8 of the Core Strategy seeks development to be located away from areas affected by air pollution unless the development incorporates appropriate mitigation measures to limit the adverse effects on occupiers and other appropriate receptors. The proposal should not result in unacceptable levels of air pollution. This is reflected in the National Planning Policy Framework which also goes on to require any new development in Air Quality Management Areas and Clean Air Zones is consistent with the local air quality action plan.
- 17.2 The Council has adopted the Low Emission Strategy on a corporate basis, which is a local air quality action plan incorporating initiatives to be delivered by the Council and will set the context for revising the Local Development Plan Policies. Measures in the Low Emission Strategy include reducing traffic, requiring electric charging points, and low emission boilers within new developments. The Low Emission Strategy is a material planning consideration, but it does not form part of the current local development plan.
- 17.3 The application was accompanied by an Air Quality Assessment which has been considered by the Environmental Quality Officer. There are two areas of consideration, construction phase and operational phase.
- 17.4 In respect of the construction phase there are no objections, the applicant proposes to follow air quality guidance in this process and the building materials are principally steel and cladding which represent low risk materials. No objections are raised but a condition is recommended for a construction management plan to ensure there are no impacts from contractors.
- 17.5 In respect of operational impacts there is not expected to be any exposure risk due to the proximity of residential receptors to the yard and car park area. The lack of impact assumes that there would be no HGVs travelling west on Bath Road. The following mitigation proposals are requested to ensure there would be no adverse air quality impact:
- Electric vehicle re-charging infrastructure should be provided in line with table 7 of the LES Technical Report. Electric vehicle charging points should be provided for 4 parking spaces as specified in the TS.
  - An exit route restriction shall be imposed to allow for eastbound HGV movements only. This can be secured through a legal agreement with the applicant.

- Should any emission producing plant be introduced, an assessment of its impact on nearby receptors should be produced.
- Construction Environmental Management Plan (CEMP) shall be produced and submitted to SBC for approval prior to commencement of works. It should include details of noise and dust control.
- The CEMP shall include non-road mobile machinery (NRMM) controls in line with table 10 of the LES Technical Report
- All construction vehicles shall meet a minimum Euro 6/VI Emission Standard.
- All heating systems shall meet the emission standards laid out in table 7 of the LES Technical Report.

17.6 Therefore, no objections are raised in respect of Air Quality impacts subject to the implementation of development in accordance with the approved plans. It should also be noted that, as set out in the Highways section, there is a notable decrease in HGV movements at the site under the proposed use.

## 18.0 **Land contamination**

18.1 Core Policy 8 (Sustainability and the Environment) of the SBC's Core Strategy Document states that development shall not 'cause contamination or deterioration in land, soil or water quality' nor shall development occur on polluted land unless appropriate mitigation measures are employed.

18.2 No objection is raised by the Council's Environmental Services Contamination Officer following considerations of the Geo-Environmental Assessment and Preliminary Risk Assessment submitted with the application.

18.3 The conclusions of the assessment require the submission of a quantitative risk assessment, site specific remediation strategy and remediation validation reports before the site can be developed and these reports can be secured by condition and are included as part of the recommended conditions for the application.

## 19.0 **Equalities Considerations**

19.1 The Council is subject to the Public Sector Equality Duty in section 149 of the Equality Act 2010, which (amongst other things) requires the Council to have due regard to the need to eliminate discrimination/harassment/victimisation, advance equality of opportunity between people who share (and do not share) a protected characteristic and foster good relations between people who share (and do not share) a protected characteristic.

19.2 Having due regard to the need to advance equality of opportunity between persons who share (and do not share) a relevant protected characteristic

involves having due regard, in particular, to the need to: (i) remove or minimise disadvantages suffered by persons who share a relevant protected characteristic that are connected to that characteristic; (ii) take steps to meet the needs of persons who share a relevant protected characteristic that are different from the needs of persons who do not share it; and (iii) encourage persons who share a relevant protected characteristic to participate in public life or in any other activity in which participation by such persons is disproportionately low.

- 19.3 Having due regard to the need to foster good relations between persons who share (and do not share) a relevant protected characteristic involves having due regard, in particular, to the need to: (i) tackle prejudice; and (ii) promote understanding.
- 19.4 The protected characteristics referred to in the Act are age, disability, gender reassignment, marriage and civil partnership, pregnancy and maternity, race, religion or belief, sex and sexual orientation. The PSED is a continuing duty to have regard to the objectives identified in the Act as opposed to requiring the Council to achieve any particular outcome.
- 19.5 Throughout this report, regard has been given to the needs of individuals with these protected characteristics, as required by the Act in order to understand the likely impact of the development proposal on them. Given that the duty is an ongoing one the Council will continue to have regard to it throughout the detailed design stage of this development proposal in due course.
- 19.6 The proposal would provide a new employment unit and it is shown to have a level threshold access and the first-floor office area is accessible by lift.
- 19.7 In relation to the car parking provisions, the plans show the provision of 2no accessible spaces which are closely located to access points to the building. Internal corridors are considered to be able to accommodate the needs of staff and visitors with disabilities.
- 19.8 It is considered that there would be temporary (but limited) adverse impacts upon all individuals with protected characteristics, whilst the development is under construction, by virtue of the construction works taking place. People with the following characteristics have the potential to be disadvantaged as a result of the construction works associated with the development eg: people with disabilities, maternity and pregnancy and younger children, older children and elderly residents/visitors. It is also considered that noise and dust from construction would have the potential to cause nuisances to people sensitive to noise or dust. However, measures can be incorporated

into the construction management plan to mitigate the impact and minimise the extent of the effects. This could be secured by condition should the scheme be acceptable.

19.9 In conclusion, it is considered that the needs of individuals with protected characteristics have been fully considered by the Local Planning Authority exercising its public duty of care, in accordance with the Equality Act 2010.

## **20.0 Presumption in favour of sustainable development**

20.1 Paragraph 11 of the NPPF states that there should be a presumption in favour of sustainable development.

20.2 In consideration of whether or not development is sustainable, para 8 of the NPPF set out 3 objectives that should be met in order for a scheme to be considered sustainable development; the economic, social and environmental objective.

20.3 In terms of the economic objective, the proposal creates a single commercial building that will support a business within the borough and result in job creation both temporary (during construction) and long-term (operational).

20.4 In terms of the social objective, it is considered to be met through the potential to bring in local construction jobs and to provide employment for people within the borough, therefore contributing to supporting a wider community.

20.5 In terms of the environmental role, the proposal is considered to be able to demonstrate very special circumstances for development in the Green Belt principally due to the existing character of the site resulting in the part of the site which is within the Green Belt is not performing the function of Green Belt Land. Similarly, the redevelopment of the site will not result in any detriment to the role of the Strategic Gap as the site does not contribute to this separation as existing. There are environmental enhancements through the provision of soft landscaping and financial contribution towards enhancements at Colne Valley Park.

20.6 There are impacts on the amenity of neighbouring residents, including potential future residents, however the impacts are not considered to be significant adverse.

## **21.0 PART C: RECOMMENDATION**

21.1 Having considered the relevant policies set out above, comments from consultees as well as all relevant material considerations it is recommended the application be delegated to the Planning Manager for approval subject to the completion of a Section 106 Agreement to secure a travel plan monitoring, financial contribution towards improvements to

Colne Valley Park and a HGV routing and management plan which are required to mitigate the impacts of development and the following conditions listed below.

## **27.0 PART D: CONDITIONS**

### **27.1 CONDITIONS:**

#### **1. Time Limit**

The development hereby permitted shall be commenced within three years from the date of this permission.

**REASON** To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

#### **2. Approved Plans**

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved unless otherwise agreed in writing by the Local Planning Authority.

- (a) Drawing No. 21-141-PL-03 Rev D, dated 10/2022, received 14/08/2023.
- (b) Drawing No. 21-141-PL-04 Rev D, dated 10/2022, received 14/08/2023.
- (c) Drawing No. 21-141-PL-01 Rev B, dated 10/2022, received 09/01/2023.
- (d) Drawing No. 21-141-PL-02 Rev A, dated 10/2022, received 09/01/2023.
- (e) Drawing No. 21-141-PL-05 Rev A, dated 10/2022, received 09/01/2023.
- (f) Drawing No. 21-141-PL-08, dated 10/2022, received 09/01/2023.
- (g) Drawing No. 21-141-PL-09, dated 10/2022, received 09/01/2023.
- (h) Drawing No. 832-ESC-00-ZZ-DR-E2100, Rev P1, dated 26/09/2022, received 09/01/2023

**REASON** To ensure that the site is developed in accordance with the submitted application and does not prejudice the amenity of the area, so as to comply with the Policies in the Development Plan.

#### **3. Materials**

Prior to the commencement of any above ground works, details of all facing materials, including where relevant, render colours, glazed facades, timber louvres and metal framework framing to be used on the relevant block on all external facades and roofs of the buildings, shall be submitted to and approved in writing by the Local Planning Authority. Samples shall be displayed on site for inspection prior to works commencing on the relevant part of the development. No part of the development shall be used or occupied prior to the implementation of the approved details. The development shall be carried out strictly in accordance with the approved details.



REASON: To ensure a satisfactory external appearance of the development and in accordance with Policy EN1 of the Local Adopted Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2023).

#### 4. Means of Access

No part of the development shall be occupied until the new means of access has been sited and laid out in accordance with the approval plans and constructed in accordance with Slough Borough Council's Design Guidance.

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development, in accordance with Core Policy 7 of the Slough Local Development Framework Core Strategy 2006-2026.

#### 5. Parking

The scheme for parking and manoeuvring indicated on the submitted plans shall be laid out prior to the initial occupation of the development hereby permitted and that area shall not thereafter be used for any other purpose.

REASON: To enable vehicles to draw off, park and turn clear of the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway.

#### 6. Electric Vehicle

Prior to the first occupation of the development hereby approved, the electric car charging shall be provided, to include a total of 4 electric vehicle charging points. The electric vehicle charging points must have a 'Type 2' socket and be rated to at least 3.6kW 16amp 0 7kW 30amp single phase, in accordance with details that have first been submitted to and approved in writing by the Local Planning Authority.

REASON: To ensure vehicle parking is provided and encourage up-take of electric vehicle use, in accordance with Policy T2 of the Adopted Local Plan (2004), Policies 7 and 8 of the Core Strategy 2008, the guidance contained in the Council's Developer's Guide Part 3 (2008) and the National Planning Policy Framework 2023

#### 7. Cycle Parking

The development hereby approved shall not be occupied until details of the cycle parking provision (including location, housing and cycle stand details) have been submitted to and approved in writing by the Local Planning

Authority. The cycle parking shall be provided in accordance with these details prior to the occupation of the development and shall be retained at all times in the future for this purpose.

REASON: To ensure that there is adequate cycle parking available at the site in accordance with Policy T8 of The Local Plan for Slough 2004, and to meet the objectives of the Slough Integrated Transport Strategy

#### 8. Travel Plan

The approved development shall be implemented in accordance with the measures and targets set out in the Framework Travel Plan (dated 2nd July 2021).

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway, to reduce travel by car in accordance with Policy T15 of the Slough Local Plan 2004 and to meet the objectives of the Slough Integrated Transport Strategy.

#### 9. Construction Management Plan

Prior to the commencement of the development hereby approved, a Construction Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The Construction Management Plan shall include the following details:

1. A site set up plan displaying hoarding/fencing extents, vehicle and pedestrian access points during construction, provision for storage of materials, waste and recycling facilities/areas, contractor parking, turning space for construction vehicles, unloading area for deliveries, site office and wheel cleaning facilities during the construction period.
2. Construction vehicles and to comply with Euro VI Emissions Standard as a minimum and machinery to comply with Table 10 of the Low Emissions Strategy Guidance.
3. Delivery hours and working hours. Deliveries shall be made outside peak hours of 0800 – 0900 and 1700 – 1800, and outside of 1430 – 1530 where the development is located in proximity to a school.
4. Details of traffic management measures to control deliveries to site and pedestrian movements on footways in proximity to the site in order to minimise the impact of construction on the safe operation of the surrounding highway network.
5. Vehicle routing plan for HGVs. HGVs shall avoid weight restrictions and AQMAs and local schools at collection/drop off time.
6. Details of dust control measures and wheel washing facilities to be provided on site.
7. Confirmation of whether any abnormal loads will be required for the construction or demolition. If so, the LHA must be notified of any abnormal loads at the following location:  
<https://www.slough.gov.uk/licences-permits/abnormal-loads/1>.

8. Confirmation that all non-road mobile machinery (NRMM) controls in line with table 10 of the LES Technical Report.
9. Confirmation that all construction vehicles shall meet a minimum Euro 6/VI Emission Standard

The plan shall thereafter be implemented as approved before development begins and be maintained throughout the duration of the construction works period.

REASON: In the interest of minimising danger and inconvenience to vehicular traffic and pedestrian highway users in accordance with policies 7 and 8 of the Core Strategy 2008 and the requirements of the National Planning Policy Framework 2023.

#### 10. Visibility Splays

No other part of the development shall be occupied until the visibility splays shown on the approved drawings have been provided on both sides of the access and the area contained within the splays shall be kept free of any obstruction exceeding 600 mm in height above the nearside channel level of the carriageway.

REASON: To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access.

#### 11. Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy (SBC Environment Officer)

No development aside from demolition of the existing buildings and clearance shall take place until a quantitative risk assessment has been prepared for the site, based on the findings of the intrusive investigation. The risk assessment shall be prepared in accordance with the Contaminated Land report Model Procedure (CLR11) and Contaminated Land Exposure Assessment (CLEA) framework, and other relevant current guidance. This must first be submitted to and approved in writing by the Local Planning Authority and shall as a minimum, contain, but not limited to, details of any additional site investigation undertaken with a full review and update of the preliminary Conceptual Site Model (CSM) (prepared as part of the Phase 1 Desk Study), details of the assessment criteria selected for the risk assessment, their derivation and justification for use in the assessment, the findings of the assessment and recommendations for further works. Should the risk assessment identify the need for remediation, then details of the proposed remediation strategy shall be submitted in writing to and approved by the Local Planning Authority. The Site Specific Remediation Strategy (SSRS) shall include, as a minimum, but not limited to, details of the precise location of the remediation works and/or monitoring proposed, including earth movements, licensing and regulatory

liaison, health, safety and environmental controls, and any validation requirements.

REASON: To ensure that potential risks from land contamination are adequately assessed and remediation works are adequately carried out, to safeguard the environment and to ensure that the development is suitable for the proposed use and in accordance with Policy 8 of the Core Strategy 2008.

#### 12. Remediation Validation (SBC Environment Officer)

No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy condition shall be occupied until a full validation report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority. The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Site Specific Remediation Strategy condition above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation from a Building Control Regulator that all such measures have been implemented.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health and in accordance with Policy 8 of the Core Strategy 2008.

#### 13. Use Class

The offices hereby permitted shall be used ancillary to the main use only and shall at no time be used as independent offices falling within Class E of the Town and Country Planning (Use Classes) Order 1987 (and in any provision equivalent to the Class in any statutory instrument revoking or re-enacting that order) and for no other purpose.

REASON In order ensure that independent offices which are high intensive employment generating uses, are directed towards the Town Centre to promote the long-term vitality and viability of the Town Centre, and to ensure offices are located in the most sustainable locations, in order to comply with Core Policies 1, 5 and 7 of The Core Strategy 2008, and the requirements of the National Planning Policy Framework 2023.

#### 14. Use Class

Notwithstanding the provisions of Schedule 2, Part 3 of The Town and Country Planning (General Permitted Development) Order 2015 (as amended), the development shall only be used for purposes falling within E(g)(iii), B2, and B8 of the Town and Country Planning (Use Classes) Order 1987 (and in any provision equivalent to the Class in any statutory instrument revoking or re-enacting that order) and for no other purpose.

REASON In order protect the amenities of the area and to ensure an appropriate use within a defined business area comply with Core Policies 5 and 8 of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2023.

#### 15. Use Class

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) (or any Order revoking or re-enacting that Order with or without modification), the land uses hereby permitted falling within E(g)(iii), B2, and B8 as defined by Town and Country Planning (Use Classes) Order 1987 (as amended) (or any Order revoking or re-enacting that Order with or without modification) shall not be used for any residential purposes falling with the C3 Use Class as defined by Town and Country Planning (Use Classes) Order 1987 (as amended) ) (or any Order revoking or re-enacting that Order with or without modification).

REASON: to prevent the loss of employment uses and to prevent poor living conditions by virtue of using of buildings which are not design coded for residential purposes in accordance with Core Policies 4, 5, and 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the National Planning Policy Framework 2023.

#### 16. Floor Space

No additional floor space created by internal sub-division, mezzanine floor, or external extension shall take place at any time.

REASON: In the interest of ensuring there is adequate parking provision for the proposed B8 and B2 uses and to protect from overspill parking on the public highway site in accordance with the objectives of the Slough Local Transport Strategy, Policy T2 of the Local Plan for Slough 2004, Core Policies 7 and 8 of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2023.

#### 17. BREEAM

The development hereby approved shall be carried out in accordance with the Sustainability Statement by Coltham, dated 24/11/2022 and BREEAM Pre-assessment Report by ESC dated 06/2022. Prior to the first use of the development hereby approved a Design Stage Certificate shall be submitted to and approved by the Local Planning Authority confirming that the development has been designed to achieve a standard of BREEAM Excellent (or equivalent standard).

REASON In the interest of sustainable development in accordance with Policy 8 of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2023.

## 18. BREEAM

Within 6 months of the development hereby approved being brought into first use a Post-Construction Review Certificate confirming the development hereby approved has been constructed so as to achieve a standard of BREEAM Excellent (or equivalent standard) shall be submitted to and approved in writing by the Local Planning Authority.

REASON In the interest of sustainable development in accordance with policy 8 of the Core Strategy of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2023.

## 19. Lighting

Prior to first occupation, the external lighting shall be installed in accordance with the approved plans and the details within the document 'Lighting Note' by ESC, ref ESC1832 dated 23/03/2023 and shall be retained thereafter.

REASON: To ensure the holistic implementation of development as approved in the interests of the visual amenity and character of the area in accordance with Policies EN1 and EN2 of the Adopted Local Plan for Slough 2004.

## 20. Landscaping

Notwithstanding the details submitted with the application, prior to the first occupation of the building a revised soft landscaping scheme shall be submitted to and approved in writing by the Local Planning Authority that includes amended screen planting at the northwest corner of the site. The works shall be carried out in accordance with the approved details be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

Management of the Landscaping on site shall be undertaken in accordance with the Landscape and Ecology Management Plan from fpcr dated 11/2022, received 09/01/2023.

REASON In the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

## 21. Surface Water Drainage Scheme (LLFA)

Before any above ground works commence a detailed design of surface water drainage scheme for the site based on sustainable drainage principles and an assessment of the hydrological and hydro geological context of the development should be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

The scheme shall include:

- i) Details (i.e., designs, diameters, invert and cover levels, gradients, dimensions and so on) of all elements of the proposed drainage system, to include pipes, inspection chambers, outfalls/inlets, and attenuation structures
- ii) Details of the drainage system are to be accompanied by full and appropriately cross-referenced supporting calculations which will include a 10% allowance for urban creep.
- iii) Cross sections of the control chambers (including site specific levels mAOD) and manufacturers' hydraulic curves should be submitted for all hydrobrakes and other flow control devices.
- iv) Detailed scheme for the ownership and scheduled maintenance for every element of the surface water drainage system.
- v) Confirmation of site-specific soil conditions to confirm or exclude use of infiltration solutions.

Reason: To reduce the risk of flooding both on and off site in accordance with the NPPF and Policy of the Core Strategy for ensuring the satisfactory means of surface water attenuation and discharge from the site and to ensure the future maintenance of drainage systems associated with the development.

## 22. Sustainable Drainage (LLFA)

No development shall take place until a detailed scheme for the ownership and maintenance for every element of the surface water drainage system proposed on the site has been submitted to and approved in writing by the Local Planning Authority and the maintenance plan shall be carried out in full thereafter.

- Details are required of which organisation or body will be the main maintaining body where the area is multifunctional (e.g., open space play areas containing SuDS) with evidence that the organisation/body has agreed to such adoption.
- The scheme shall include, a maintenance schedule setting out which assets need to be maintained, at what intervals and what method is to be used.
- A site plan including access points, maintenance access easements and outfalls.
- Maintenance operational areas to be identified and shown on the plans, to ensure there is room to gain access to the asset, maintain it with appropriate plant and then handle any arisings generated from the site.

- Details of expected design life of all assets with a schedule of when replacement assets may be required.

Reason: To ensure the future maintenance of drainage systems associated with the development.

### 23. Verification Report (LLFA)

No Occupation shall take place until the Verification Report for the installed surface water drainage system for the site based on the approved Flood Risk Assessment & Sustainable Drainage Strategy, Ref. P/10697/015(006) FLOOD RISK ASSESSMENT AND DRAINAGE STRATEGY has been submitted in writing by a suitably qualified drainage engineer and approved by the Local Planning Authority The report shall include:

- a) Any departure from the agreed design is keeping with the approved principles
- b) Any As-Built Drawings and accompanying photos
- c) Results of any Performance testing undertaken as a part of the application process (if required / necessary)
- d) Copies of any Statutory Approvals, such as Land Drainage Consent for Discharges etc.
- e) CCTV Confirmation that the surface water drainage system is free from defects, damage, and foreign objects
- f) Confirmation of adoption or maintenance agreement for all SuDS elements as detailed within the drainage strategy is in place

Reason: To ensure the installed Surface Water Drainage System is satisfactory and in accordance with the approved reports for the development site.

### 24. Bird Box

The development hereby approved shall be undertaken in accordance with the recommendations of the Ecology Appraisal by fpcr, dated 09/2022, received 09/01/2023.

Prior to the development hereby approved being brought into use, details shall be submitted to and approved in writing by the Local Planning Authority of proposals to provide bird boxes on the site. The development shall be implemented in accordance with the approved details and be in place prior to first use of the site and shall thereafter be retained.

REASON: To ensure the development provides a net gain in biodiversity in accordance with paragraph 174 of the NPPF 2023.

### 25. Arboricultural Impact Assessment

The development hereby approved shall be carried out in accordance with the recommendations of the Arboricultural Impact Assessment from aspect



Arboriculture ref: 11381\_AIA.001 Rev B, dated 11/2022, received 09/01/2023 unless otherwise approved in writing by the Local Planning Authority.

Reason: To ensure the protection of existing trees during the construction phase in the interests of the character of the area in accordance with saved policy EN1 of the Slough Local Plan.

## 26. HGV Routing and Management Plan (TBC – S.106/condition)

Prior to the development hereby approved coming into use, details shall be submitted to and approved in writing by the Local Planning Authority of a proposed HGV routing and management plan that will include:

- Plans for the introduction and management of an exit route restriction that will prohibit HGVs exiting the site and travelling westbound on Bath Road to accord with the proposals in the Air Quality Management Assessment from Air and Acoustic Consultants ref 100563, dated 05/10/2022.
- Plans for the implementation and management of restrictions for non-electric HGV (greater than or equal to 25 tonnes) movements at an average of 12 individual movements (arrivals and departures) in any hour during daytime hours between 07:00 and 23:00, and an average of 4 movements in any hour during night time hours between 23:00 and 07:00 in accordance with the Noise Assessment from Air and Acoustic Consultants ref: 100563 version 02, dated 11/08/2022.

The works shall be carried out, and the site shall operate, in accordance with the approved details and thereafter be retained throughout the life of the development unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the development does not have an adverse impact on Air Quality and Noise amenity in accordance with saved policy EN1 of the Slough Local Plan, Policy 8 of the Core Strategy 2006-2026 and the National Planning Policy Framework 2023.

## 27. Plant Equipment

Notwithstanding the details in the approved plans, details of any external plant, equipment and/or machinery to be installed at the site shall be submitted to and approved in writing by the Local Planning Authority. Details shall include a noise assessment that demonstrates that the plant noise rating level would not exceed the background noise level at the nearest noise sensitive receptor. Works shall be installed in accordance with the approved details and retained in a working condition at all times in the future.

REASON To prevent air pollution of the protect the amenities of the local residents in accordance with Policy EN29 of The Local Plan for Slough 2004.

## 28. Noise

Throughout the occupation of the development, monitoring shall be undertaken by the site occupier of internal noise levels to ensure that these shall be less than the specified levels in the AAC Noise Assessment (100563[02]\_Colnbrook\_Slough\_Noise\_Assessment\_Final dated 11/08/2023). Monitoring shall be averaged at intervals along a line inside the south and west facades during the daytime (07:00-23:00) and the night-time period (23:00-07:00). The averaging period for the measurements should be 1 hour during the daytime and 15 minutes during the night-time. The results of monitoring shall be made available to the Council upon request.

Reason: To ensure the development does not have an adverse impact on noise amenity in accordance with saved policy EN1 of the Slough Local Plan, Policy 8 of the Core Strategy 2006-2026 and the National Planning Policy Framework 2023.

### **Informatives:**

Thames Water

#### Waste Comments

There are public sewers crossing or close to your development. If you're planning significant work near our sewers, it's important that you minimize the risk of damage. We'll need to check that your development doesn't limit repair or maintenance activities, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. <https://www.thameswater.co.uk/developers/larger-scale-developments/planning-your-development/working-near-our-pipes>

Thames Water would advise that with regard to FOUL WATER sewerage network infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

The application indicates that SURFACE WATER will NOT be discharged to the public network and as such Thames Water has no objection, however approval should be sought from the Lead Local Flood Authority. Should the applicant subsequently seek a connection to discharge surface water into the public network in the future then we would consider this to be a material change to the proposal, which would require an amendment to the application at which point we would need to review our position.

A Trade Effluent Consent will be required for any Effluent discharge other than a 'Domestic Discharge'. Any discharge without this consent is illegal and may result in prosecution. (Domestic usage for example includes -

toilets, showers, washbasins, baths, private swimming pools and canteens). Typical Trade Effluent processes include: - Laundrette/Laundry, PCB manufacture, commercial swimming pools, photographic/printing, food preparation, abattoir, farm wastes, vehicle washing, metal plating/finishing, cattle market wash down, chemical manufacture, treated cooling water and any other process which produces contaminated water. Pre-treatment, separate metering, sampling access etc may be required before the Company can give its consent. Applications should be made at <https://wholesale.thameswater.co.uk/Wholesale-services/Business-customers/Trade-effluent> or alternatively to Waste Water Quality, Crossness STW, Belvedere Road, Abbeywood, London. SE2 9AQ. Telephone: 020 3577 9200.

Thames Water would recommend that petrol / oil interceptors be fitted in all car parking/washing/repair facilities. Failure to enforce the effective use of petrol / oil interceptors could result in oil-polluted discharges entering local watercourses.

### Highways

The development must be so designed and constructed to ensure that surface water from the development does not drain onto the highway or into the highway drainage system.

The applicant is advised that if it is intended to use soakaways as the method of dealing with the disposal of surface water then the permission of the Environment Agency will be necessary.

The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding, skip or any other device or apparatus for which a licence must be sought from the Highway Authority.

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Registration Date:	29-Jun-2023	Application No:	P/00072/128
Officer:	Martin Cowie	Ward:	
Applicant:	Mr. Martin Butcher, Equinix, Inc.	Application Type:	Major
		13 Week Date:	28 September 2023
Agent:	Mr. Robert Purton, David Lock Associates 50 North Thirteenth Street, Central Milton Keynes, Milton Keynes, MK9 3BP		
Location:	Akzonobel Decorative Paints, Wexham Road, Slough, SL2 5DB		
Proposal:	Planning application for the decontamination of the site, demolish the remaining buildings, undertake ancillary engineering work to stabilise the ground conditions and the delivery of ancillary retaining walls (amended description and details submitted)		

**Recommendation:** Delegate to the Planning manager for Approval



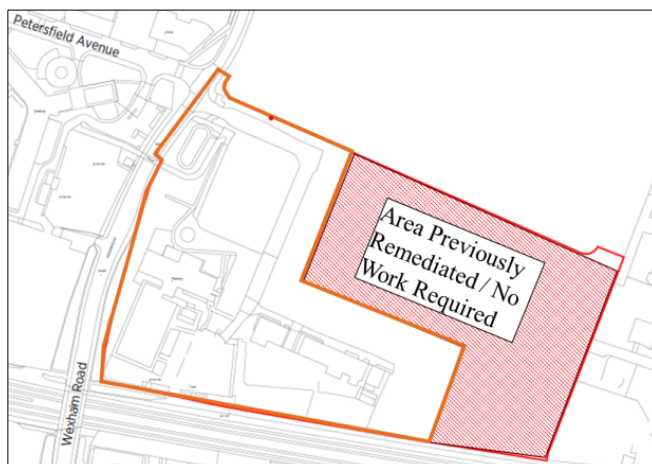
## **SUMMARY OF RECOMMENDATION**

- 1.0 This application has been referred to the Planning Committee for consideration as the application is for a major development (the site area is over 1 hectare).
- 1.1 Having considered the relevant policies set out below, and comments that have been received from consultees, and all other relevant material considerations it is recommended the application be delegated to the Planning Manager:
- A) For approval subject to finalising conditions, informatives, and any minor changes.

## **PART A: BACKGROUND**

### **2.0 Proposal**

- 2.1 As per the description of development, this application seeks full planning permission for:
- The decontamination of the site, demolish the remaining buildings, undertake ancillary engineering work to stabilise the ground conditions and the delivery of ancillary retaining walls (amended description and details submitted)
- 2.2 These 'enabling' works seeks to prepare the site, formerly in use as the AkzoNobel paint and manufacturing facility for a future commercial redevelopment. Equinix, the owners of the site are currently exploring the delivery of a data centre campus to secure a long-term investment in this location. Any such development proposal would be subject to a Major planning application for detailed consideration, and a decision by the Local Planning Authority. A decision in respect to the current application for site preparatory works, would not however prejudice any future decision in relation to an application for the development and use of the site.
- 2.3 This works application will effectively facilitate the completion of the demolition and decontamination of this remaining part of site following similar works allowed under a Prior Approval application in 2019 (see site plan and site history below).



Site Plan

- 2.4 The proposed works comprise the following key elements:
- Demolition of the existing buildings, structures and hardstanding
  - Completion of the decontamination of the site
  - Levelling and stabilising ground conditions including making safe of underground infrastructure and provision of temporary drainage
  - Construction of ancillary retaining wall adjacent Wexham Road and replacement of existing wall at underpass

- 2.5 A comprehensive set of supporting documentation has been submitted with the application detailing the nature of the proposed works, their impact and associated mitigation measures. The key documents include:

- Topographical Surveys
- Demolition Scope Plan
- Proposed Site Levels and Layout Plan
- Proposed Site Sections
- Proposed Retaining Wall Plan
- Underpass Remedial Works Report
- Phase 1 Desk Study
- Phase II Geo-Environmental Assessment Report
- Remediation Strategy
- Arboricultural Impact Appraisal and Method Statement
- Enabling Works Tree Protection Plan
- Outline Demolition Method Statement
- Preliminary Ecological Appraisal
- Phase 1 Habitat Survey
- Bat Survey Reports and Badger Survey Report
- Preliminary Biodiversity Metric Assessment Technical Note
- Cultural Heritage Desk Based Assessment
- Detailed Unexploded Ordnance Assessment
- Archaeological Written Scheme of Investigation
- Flood Risk Assessment and Drainage Strategy

*Demolition works:*

- 2.6 The scope of the demolition works on site include the removal of the existing buildings and ground floor slabs and the breaking down of existing foundations and associated road infrastructure. There are six former industrial buildings to be removed, including a single storey fuel store, a 5-storey and a 2-storey office building together with further 2 and 3-storey industrial buildings. The buildings were in use as offices, labs, and manufacturing facilities but have not been occupied as part of the paint and manufacturing use for several years and are currently vacant.
- 2.7 The buildings to be demolished are show in the diagram and photographs below. A Demolition Scope of Works and an outline Demolition Method Statement have been submitted which set out how the buildings and related infrastructure will be demolished and removed and the land reinstated temporarily.



Green buildings are proposed to be demolished. Those highlighted in red are buildings which have already been demolished.



Photographs of buildings to be demolished

**Decontamination:**

- 2.8 Contamination on part of the wider site has already been appropriately treated. The remaining area of the site, subject of this application will similarly entail the appropriate removal and treatment of contamination as part of the proposed demolition and earthworks.
- 2.9 The application is supported by a Phase 1 Desktop Study, a Phase II Geo-Environmental Assessment Report and a Remediation Strategy. These documents provide information on the previously approved and undertaken decontamination work and align this approach with the scope and need for concluding the remaining site clearance and decontamination.

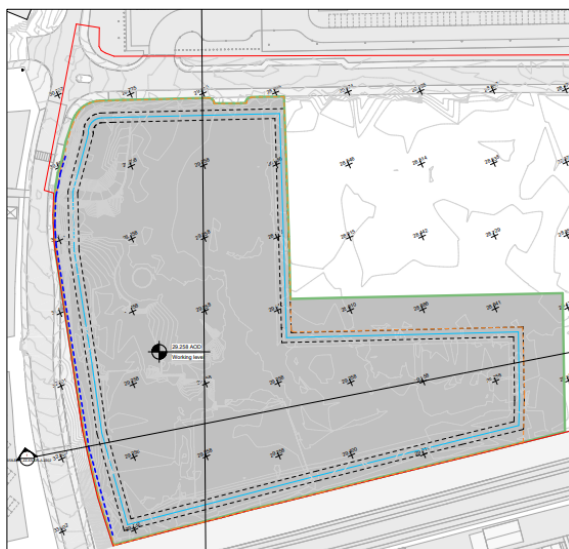


*Levelling and stabilising ground conditions:*

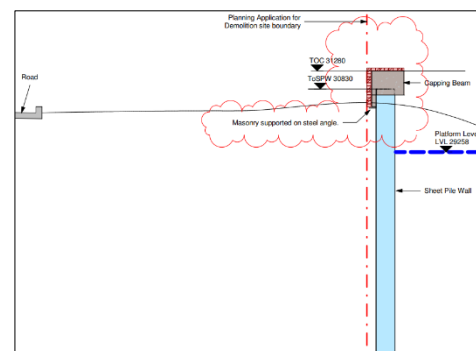
- 2.10 The proposed works will involve the moving, removal, treatment, crushing and levelling of existing ground material and earth. As part of this process underground infrastructure and services and the adjacent railway will be protected, and appropriate drainage provided to ensure that the land is kept in a stable and safe condition in readiness for any future re-purposing and re-development. In addition, it should be noted that any archaeological features present and matters of ecological importance including trees are proposed to be safeguarded.
- 2.11 Prior to the commencement of works, temporary barriers will be installed along the site boundary and maintained for the duration of the construction works.

*Retaining wall on western boundary:*

- 2.11 In seeking to achieve the appropriate ground levels across the site, which are intended to be lower than existing, a sheet pile retaining wall is proposed along its western boundary adjoining Wexham Road. The wall will accommodate the loadings from the adjacent footpath and highway and consists of driving sheet piles to the required depth along the site boundary with some backfill and constructing a concrete capping beam on top and faced in brickwork to the exposed elevation fronting Wexham Road. Within the site where the ground levels are much lower, the exposed part of the retaining wall would be finished in smooth concrete.
- 2.12 The proposed retaining wall will be 80cm metres at its lowest point and 1.7m metres at its highest point at the site boundary on the Wexham Road side, separated by sloping verge between the footpath and carriageway. The location and detail of the wall is illustrated in the diagrams below.



Wall – dotted purple line on western boundary of site



Section of wall in relation to Wexham Road

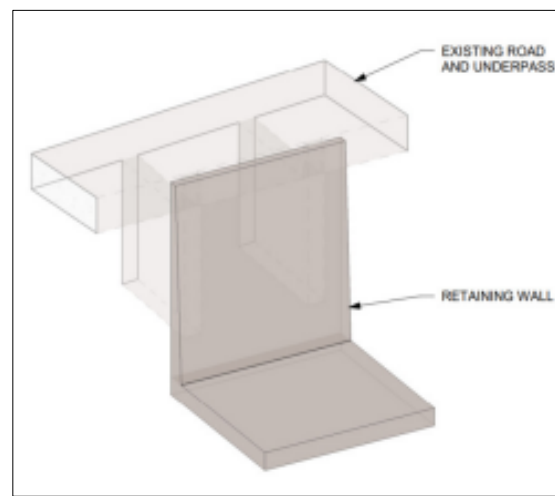
*Underpass retaining wall:*

- 2.13 As part of the structural solution to negate disruption to Wexham Road, the existing retaining wall located at the underpass in the north-western corner of the site is also proposed to be removed and replaced with a new wall. The proposed works will remain within the site owned by Equinix and will not extend beyond the site

boundary. The location and detail of the proposed wall is shown below.



Location of existing ramped underpass



3D view of the proposed retaining wall

- 2.14 Edge protection will be provided around the underpass with restricted access to the work area as a safety precaution. As the extent of the works will be kept within the site boundaries, the underpass beneath Wexham Road will not be impacted.
- 2.15 It is anticipated that works will commence in January 2024 and be completed by December that year.

### 3.0 **Application Site**

- 3.1 The application site lies in a long-established industrial area into which more recent commercial uses have been introduced. The site is bounded by Wexham Road to the west, the railway running between Slough and Langley to the south, Uxbridge Road to the east and a new access road, off Wexham Road serving the application site to the north. Directly opposite this access road, two future data centres are currently being constructed for Yondr (see Site History below) on land that was also formerly part of the AkzoNobel paint coating and manufacturing site. AkzoNobel headquarters are located on the opposite side of Wexham Road to the west in a four-storey building accommodating offices and laboratories.
- 3.2 Across the Great Western Railway to the south and at some distance from the site is an area of mainly two-storey traditional houses accessed on Colonial Road, Australia Road, Canada Road, and India Road.

3.3



3.4 *Aerial photograph of application site (green shading denotes new access road)*

3.5

The site measures approximately 5.1 hectares including the access road referred to above and the area specifically subject to the works as part of this application measures approximately 2.1 hectares. The other, eastern half of the site formed part of a Prior Approval submission for an earlier phase of enabling works involving demolition, de-contamination and other associated activities which have already been carried out.

3.6

The application site currently accommodates six vacant former industrial buildings including a single storey fuel store, a 5-storey and a 2-storey office building together with further 2 and 3-storey industrial building. An underpass and ramp (under Wexham Road) also form part of the application site and some tree by the southern boundary.

It should be noted that to the east of the site is land occupied by the gas supplier Cadent. This formally comprised a storage depot, however it is currently being redeveloped to rationalise Cadent's operations to the north and once completed, the land is expected to become vacant.

The site is located within a defined Business Area in planning policy terms and represents previously developed land formerly used for General Industrial and Research and Development purposes (Use Class B2).

3.7

The application site benefits from an outline planning permission to for up to 1,000 dwellings (see Site History below). No reserved matters application however in relation to this consent have come forward.

#### 4.0 **Site History**

4.1 The most relevant planning history for the site is presented below:

P/00072/092 Application for the prior approval for the demolition of established industrial area (and restoration of the site).

Approved with conditions; Informatives – 15 July 2019

#### **[Implemented]**

P/00072/096 Outline planning application (to include matter of principal points of access), to be implemented in phases, for mixed use development comprising:

- a) Demolition of existing buildings and structures and preparatory works (including remediation) and access from Wexham Road;
- b) up to 1,000 residential dwellings (Use Class C3); along with flexible commercial uses including all or some of the following use classes A1 (Shops), A2 (Financial and Professional Services), A3 (Food and Drink), D1 (Non-residential Institutions) and D2 (Assembly and Leisure); car parking; new public spaces, landscaping; vehicular and pedestrian access; and
- c) the provision of commercial floorspace including all or some of the following use classes B2 (General Industry), B8 (Storage or Distribution) and sui generis data centre (including ancillary office space and associated plant and infrastructure provision); car parking, landscaping and vehicular and pedestrian access.

(Matters of Scale, Layout, Appearance, and Landscaping to be dealt with by reserved matters).

Approved with Conditions; Informatives – 19 November 2020

#### **[Part Implemented]**

P/17377/001 Screening Opinion for - Outline and full details applications for the redevelopment of the former ICI Paint Manufacturing facility for the construction of new commercial and residential floorspace, along with associated parking, new/improved pedestrian and cycle links, means of enclosure, landscape infrastructure and associated engineering operations including retaining structures, earthworks and drainage.

EIA Required – 20 August 2019

## **Neighbour site to the north:**

P/00072/108 Approval of reserved matters following the outline approval reference P/00072/096 dated 19th November 2020 for the mixed use development of land at the former Akzonobel Decorative Paints facility, Wexham Road, Slough SL2 5DB. Reserved matters application for full details of access (internal site arrangements), appearance, layout, scale, and landscaping for the first phase of the approved commercial floorspace, comprising data centre use (including ancillary office space and associated plant and infrastructure provision); car parking, landscaping and vehicular and pedestrian access.

Approved with Conditions; Informatives – 12 October 2021

P/00072/118 Variation of condition 1 (Approved Plans) of planning permission P/00072/108 dated 12/10/2021 to provide minor changes to the layout of the site and appearance of the buildings.

Approved with Conditions; Informatives – 16 December 2022

**[Implemented]**

## **5.0 Neighbour Notification**

5.1 In accordance with Article 15 of The Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended), a number of site notices were displayed around the site on October 2023 and the application was advertised as a Major application on 11<sup>th</sup> July 2023 in The Slough Express on 21<sup>st</sup> July 2023. Following the submission of additional and amended information/plans further site notices were displayed on 10th October 2023 and another press notice published in The Slough Express on 5<sup>th</sup> October 2023.

5.2 No neighbour representations have been received at the time of writing this report.

## **6.0 Consultation**

### **6.1 Local Highway Authority:**

No objections subject to conditions following submission of additional information.

### **6.2 Environmental Quality (Noise and Air):**

No objections subject to safeguarding conditions covering the submission of a detailed Demolition Environmental Management Plan and Dust Management Plan.

### **6.3 Environmental Quality (Land Contamination):**

Relevant site investigation reports and Remediation Strategy (assessing the extent of contamination and the risks to the proposed development, its users or the wider

environment) acceptable. Condition requiring the submission of a Remediation Validation report recommended.

6.4 Lead Local Flood Authority (Hampshire):

No objections subject to conditions.

6.5 Thames Water:

No objections.

6.6 Environment Agency:

No objections subject to conditions.

6.7 Network Rail

No objections subject to conditions.

6.8 Health and Safety Executive

No objections.

6.9 Cadent

No comments received. Update to be reported at Committee via Amendment Sheet.

6.10 Berkshire Archaeology:

No objections subject to conditions.

**PART B: PLANNING APPRAISAL**

7.0 **Policy Background**

7.1 The following policies are considered most relevant to the assessment of this application:

The National Planning Policy Framework (NPPF) 2023

The relevant chapters within the National Planning Policy Framework are:

Chapter 2. Achieving sustainable development

Chapter 4. Decision-making

Chapter 6: Building a strong, competitive economy

Chapter 8. Promoting healthy and safe communities

Chapter 9. Promoting sustainable transport

Chapter 11. Making effective use of land  
Chapter 12. Achieving well-designed places  
Chapter 14: Meeting the challenge of climate change, flooding and coastal change  
Chapter 15: Conserving and enhancing the natural environment  
Chapter 16: Conserving and enhancing the historic environment

Paragraph 11 of the NPPF states that decisions should apply the presumption in favour of sustainable development which means:

- c) approving development proposals that accord with an up-to-date development plan without delay; or
- d) where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date granting permission unless:
  - i. the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed (footnote 7); or
  - ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

Footnote 7 notes that the policies referred to are those in the NPPF (rather than those in development plans) relating to: habitats sites (and those sites listed in paragraph 180) and/or designated as Sites of Special Scientific Interest; land designated as Green Belt, Local Green Space, an Area of Outstanding Natural Beauty, a National Park (or within the Broads Authority) or defined as Heritage Coast; irreplaceable habitats; designated heritage assets (and other heritage assets of archaeological interest referred to in footnote 67); and areas at risk of flooding or coastal change.

The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, (December 2008)

Core Policy 1 – Spatial Strategy  
Core Policy 5 – Employment  
Core Policy 7 – Transport  
Core Policy 8 – Sustainability and the Environment  
Core Policy 9 – Natural and Built Environment  
Core Policy 11 – Social Cohesiveness  
Core Policy 12 – Community Safety

The Local Plan for Slough, Adopted March 2004

EN1 – Standard of Design  
EN3 – Landscaping Requirements  
EN5 – Design and Crime Prevention  
EN6 - Interference with Telecommunication Signals

EN22 - Protection of Sites with Nature Conservation Interest  
EN24 - Protection of Watercourses  
EN34 - Utility Infrastructure  
EMP2 - Criteria for Business Developments  
EMP12 - Remaining Existing Business Areas  
T2 – Parking Restraint  
T8 – Cycle Network and Facilities  
T9 - Bus Network and Facilities

#### Slough Local Development Plan and the NPPF

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework.

The revised version of the National Planning Policy Framework was published on 5<sup>th</sup> September 2023.

The relevant Local Development Plan Policies in relation to determining this application are largely considered to be in compliance with the National Planning Policy Framework 2023. Any non-compliance parts are addressed in the planning assessment.

#### The Proposed Spatial Strategy (Nov 2020)

Under Regulation 18, the Proposed Spatial Strategy for the Local Plan for Slough was the subject of public consultation in November 2020. This set out a vision and objectives along with proposals for what the pattern, scale and quality of development will be in Slough. The consultation document contained a revised Local Plan Vision which supports the Council's vision for Slough as a place where people want to "work, rest, play and stay."

It should be noted that the consultation document for the Proposed Spatial Strategy does not contain any specific planning policies or allocate any sites. It made it clear that the existing planning policy framework for Slough would remain in force until replaced by new Local Plan policies in the future. Nevertheless, it sets out the most up to date statement of the Council's position with regards to strategic planning issues. As a result, it is relevant for the consideration of this application (but only very limited weight can be afforded to the specific and strategic guidance therein).

#### Equality Act

In addition, Section 149 of the Equality Act (2010) which sets a Public Sector Equality Duty (PSED) came into force in April 2011 and requires the Council to



consider the equality impacts on all protected groups when exercising its functions. In the case of planning, equalities considerations are factored into the planning process at various stages. The first stage relates to the adoption of planning policies (national, strategic and local) and any relevant supplementary guidance. In coming to a recommendation, officers have considered the equalities impacts on protected groups in the context of the development proposals as set out below in this report.

#### Other relevant documents

- Slough Local Development Framework Proposals Map 2010
- Slough Borough Council Developer's Guide Parts 1-4
- Slough Low Emission Strategy 2018 – 2025

7.2 The main planning issues relevant to the assessment of this application are considered to be as follows:

- Principle of development
- Impact on the character and appearance of the area
- Impact on amenity of neighbouring occupiers / uses
- Access, highway safety and parking
- Impact on biodiversity and ecology
- Contaminated land
- Archaeology
- Flood Risk and drainage
- Equalities considerations

#### 8.0 **Principle of Development**

8.1 The site is within a defined Business Area where the development plan seeks to retain employment generating uses. This application for 'enabling' works seeks to prepare the site, formerly in industrial use and currently redundant, for a future commercial and employment generating redevelopment. Equinix, the owners of the site are currently exploring the delivery of a data centre campus to secure a long-term investment in this location. The proposed works will effectively facilitate the completion of the demolition and decontamination of this remaining part of site following similar works allowed under a Prior Approval application in 2019.

8.2 As indicated previously in the report, any such future development proposal would be subject to a Major planning application for detailed consideration, and a decision by the Local Planning Authority. A decision in respect to the current application for site preparatory works, would not however prejudice any future decision in relation to an application for the development and use of the site for appropriate employment generating purposes.

#### 9.0 **Impact on the character and appearance of the area**

9.1 Policies EN1 and EMP2 of the Adopted Local Plan for Slough and Core Policy 8 of the Core Strategy require development to be of a high standard of design which

respects, is compatible with and/or improves and the character and appearance of the surrounding area. Chapter 12 of the National Planning Policy Framework states “the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve”.

- 9.2 The application proposes enabling works to complete the preparation of the site for redevelopment. They involve the removal of a number of old industrial buildings, of no particular architectural merit, ground works to level the site and the construction of retaining walls. Apart from the existing buildings, the site which has already been partly cleared, comprises bare ground, hardstanding and scrub/grassland. There are a number of scattered trees of low quality outside the western and southern boundaries of the site. The surrounding land is subject to redevelopment with a large data centre currently under construction immediately to the north.
- 9.3 In its present state, the site has a negative impact on the character and appearance of the area and in seeking to facilitate its redevelopment, the application is considered to represent a positive step towards enhancing the townscape and environmental quality of the local area longer-term.
- 9.4 Aside from the demolition and clearance works, the most notable change is the proposed retaining wall along the western boundary of the site adjacent Wexham Road. The wall is necessary to safeguard the stability of the land in this location and whilst it would stretch along most of the western boundary, it will not appear prominently given its height (varying in height from 80cm to 1.7 on the site boundary fronting Wexham Road), brick faced treatment, partial set back from Wexham Road and the urban character of the surrounding area. Given the site level changes, some temporary hoarding will be required on top of the boundary wall which will be secured by the hoarding licence procedure (this falls outside of the planning remit). While this will not be good design in terms of visual amenity, it would only be for a temporary period. A more suitable structure to replace the hoarding will be required as part of the future redevelopment of the site.
- 9.5 The existing site contains limited vegetation, with the notable exceptions of several scattered trees within its boundary and two small groups of trees just outside along its southern perimeter by the railway line. All the trees are of low quality and those along the southern boundary will be protected as stated in the Arboricultural Impact Appraisal and Method Statement and Enabling Works Tree Protection Plan. In landscape terms therefore, it is not considered that the proposed works will adversely affect the character and appearance of the area.
- 9.6 It is noted that a number of mature trees which lined the western boundary have been removed. These provided some landscape value in the Wexham Road streetscene. Given the nature of this enabling application it would not be reasonable to secure replacement tree planting at this stage and would be required when a planning application is submitted to best locate trees and landscaping for the longevity of the redevelopment of the site.
- 9.7 Based on the above, the proposal is therefore considered to comply with the relevant

requirements of Core Policy 8 of The Core Strategy, Policies EN1 and EMP2 of The Local Plan for Slough, and the requirements of the National Planning Policy Framework.

10.0 **Impact on amenity of neighbouring occupiers / uses**

10.1 Paragraph 130f of the National Planning Policy Framework requires planning decisions to ensure developments create places with a high standard of amenity for existing and future users. This is reflected in Core Policy 8 of the Core Strategy and Local Plan Policies EN1 and EMP2.

10.2 The site is in an established industrial area which is currently undergoing partial redevelopment. The wider area is residential in character accommodating with the nearest housing to the south of the site opposite the Great Western Railway line. It should be noted that the housing in this location is mainly orientated east to west so do not have a direct view cross to the site.

10.3 The application is accompanied by a range of supporting documents outlining how the enabling works will be undertaken and highlighting potential impacts in respect to local environmental amenity and measures detailing how these will be addressed. These documents include an Outline Demolition Method Statement and Demolition Scope Plan covering matters in relation to hours of working, health and safety, security, noise and dust mitigation, construction and delivery vehicle routing, wheel cleansing, access and parking, and waste management including contaminated material. The approach set out seeks to minimise the impacts of the works on neighbouring amenity and the local environment.

10.4 Working hours be limited to weekdays only from 08:00 to 18:00 hours as stated in the outline Demolition Method Statement and all associated construction related traffic will abide by these hours unless otherwise agreed with the Council.

10.5 The Council's Environmental Services Officers have reviewed the application and confirmed that based on the information submitted and subject to detailed safeguarding conditions, the proposed works are acceptable in principle and that any issues arising can be managed and addressed to minimise impact on neighbouring amenity.

10.6 Based on the above, the proposal is therefore considered to comply with the relevant requirements of Core Policy 8 of The Core Strategy, Policies EN1 and EMP2 of The Local Plan for Slough, and the requirements of the National Planning Policy Framework.

11.0 **Access, highway Safety and parking**

11.1 The National Planning Policy Framework states that development should be designed to create safe and suitable access and layouts which minimise conflicts between traffic and pedestrians. Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, should

be cost effectively mitigated to an acceptable degree. This is reflected in Core Policy 7. Paragraph 111 of the National Planning Policy Framework 2023 states that 'Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe'.

- 11.2 Pedestrian, cycle and vehicular access to the site is from Wexham Road to the west and via the new road along its northern boundary. The site benefits from good road connectivity with the M4 providing easy access to the M25 and M40. As was the case with the implementation of enabling work on the eastern part of the site, during the course of the site will be accessed via a main access gate with all vehicular traffic taken from the access road to the north via Wexham Road. Pedestrian and cycle access will also be via the main gate to the north and will be separated from the vehicle access to ensure safety of all users. In order to maintain highway and site safety there will be clear signage to all access points for the duration of the work.
- 11.3 An indicative routing management plan for construction traffic has been prepared which seeks to ensure that vehicles use only the main roads, avoid residential streets and the Air Quality Monitoring Areas where possible. The preferred routing of vehicles as set out in the outline Demolition Method Statement submitted with the applications avoids traffic turning right into Wexham Road to mitigate impact on neighbouring residential areas. Considering the daily excavation rate and assumed re-use of material, from both the excavation and demolition works, it is estimated that an average of approximately 10 HGVs will be expected daily. These vehicles will be expected during construction hours only as previously indicated.
- 11.4 Due to the nature of the site and space available, car parking will be provided on site. It is envisaged that the car park will be located to the western elevation of the site (near the entry gate) within the site accommodation area as shown in the outline Demolition Method Statement. This will be secured by condition.
- 11.5 It is assumed that approximately 50% of the workforce (including managers) will access the site by a private car and will require a parking space. With an estimated peak workforce of 40, and some additional allowance for visitors and site buses, 25 parking bays have been estimated. The applicant will implement a Travel Plan that supports and encourages sustainable travel (public transport, cycling, walking, and car-sharing). Site employees and visitors will be encouraged to shift from travel by private car to active and sustainable modes of transport. It is also envisaged that the contractor will provide a site bus to bring teams from the depot.
- 11.6 The Councils Transport Officers have reviewed the application and raise no objections subject to safeguarding conditions. Based on the above, it is considered that the proposal would accord with Core Policy 7 of the Core Strategy Local Plan Policies T2 and T8, and the requirements of the National Planning Policy Framework.
- 12.0 **Biodiversity and ecology**

- 12.1 The application is supported by a Preliminary Ecological Appraisal which details the surveys that have been carried out at the site to establish the presence, if any, of protected species. Following, preliminary roost assessments in January it was established that there was a low-risk potential that buildings B1, B2 and B3 could support bats. Therefore, Bat Hibernation Surveys in February and emergence surveys in May were carried out. No evidence of bats using the buildings was found. The application is also supported by a Biodiversity Metric Assessment Technical Note which has established the baseline biodiversity of the site habitat in line with the Biodiversity Metric 4.0.
- 12.3 A badger survey has also been carried out on the site and along the southern boundary and no evidence of badgers have been found within the site. In addition, a Badger walk over survey was carried out on railway embankment land on the 15th May 2023 to confirm whether badgers may be using the land along the railway embankment which will likely fall within 30 metres of the enabling works. The walk over survey found no signs of badgers, however, as part of the embankment could not be accessed due to being overgrown the conclusions of the survey have advised that works within 30m of the area not accessed should proceed on a precautionary basis and include a working method statement to minimise risk to any potential setts.
- 12.4 The application is also supported by a Biodiversity Metric Assessment Technical Note which has established the baseline biodiversity of the site habitat in line with the Biodiversity Metric 4.0. This additionally details opportunities to achieve Biodiversity Net Gain of at least 10% through habitat creation and will be further explored and confirmed as part of any redevelopment proposal brought forward.
- 12.5 A Habitat Survey has also been submitted more recently to supplement the previously submitted Preliminary Ecological Appraisal. This was undertaken in optimum survey conditions and confirms that there are no adverse impacts on species or habitats of importance.
- 12.6 An Arboricultural Impact Appraisal and Method Statement accompanying the application identifies existing trees on site and off site on adjoining boundaries and outlines how these will be protected during the demolition and construction works, in line with standard practice and guidance.
- 13.0 **Contaminated Land**
- 13.1 Paragraphs 183 and 184 of the National Planning Policy Framework require a site to be suitable for its proposed use taking account of ground conditions and any risks arising from land instability and contamination. This is reflected in Core Policy 8 of the Core Strategy.
- 13.2 The site has been directly contaminated due to past industrial operations. This has been partly addressed with the completion of the enabling works on the eastern half on the site.

- 13.3 The application is supported by a Phase 1 Desk Top Study and a Remediation Strategy. These documents provide information on the previously approved and undertaken decontamination work and align this approach with the scope and need for concluding the remaining site clearance and decontamination.
- 13.4 The Council's Environmental Services (Contamination) Officer has reviewed the application and has raised no objections to the approach proposed subject to a safeguarding condition. The Environment Agency have also confirmed that they have no objections subject to conditions.
- 13.3 Based on the above, it is considered that the proposal would accord with Core Strategy Local Plan Policy 8 and the requirements of the National Planning Policy Framework.
- 14.0 **Flood Risk and Drainage**
- 14.1 Paragraph 167c of the National Planning Policy Framework requires the incorporation of sustainable drainage systems unless there is clear evidence that this would be inappropriate. This is also required by Paragraph 169 of the National Planning Policy Framework for all major developments. Core Policy 8 of the Core Strategy requires development to manage surface water arising from the site in a sustainable manner. The proposed drainage system has been assessed by the Lead Local Flood Authority, and after requesting further information in relation to the above ground water tanks and Thames Water agreement, have approved the proposed drainage strategy.
- 14.2 A Flood Risk Assessment and Drainage Strategy has been submitted with the application confirms that the site is within Flood Risk 1 area (low probability of flooding). It has been agreed on the eastern half of the site the removal of the retained hard surfacing and buildings will provide drainage betterment.
- 14.3 Following demolition of the existing buildings, hard surfacing and remediation, the proposed site levels will be laid out creating a permeable site. The proportional strategy previously adopted by the Council for the other part of the site is extrapolated to the remainder of the site subject to this works application. A full drainage strategy will be designed when any proposals for the redevelopment are brought forward.
- 14.4 The Council's Flood Risk Officer has reviewed the application and has raised no objections subject to conditions. The application has also been reviewed by the Environment Agency which has raised no objection subject to conditions which have been included below.
- 14.5 Based on the above, the proposal would not increase flood risk elsewhere outside of the application site, and the flood risk and drainage issues would comply with Core Policy 8 of the Core Strategy, and the requirements of the National Planning Policy Framework.

## 15.0 **Archaeology**

15.1 A Cultural Heritage Assessment and Written Scheme of Investigation for a geoarchaeological watching brief and trial trench evaluation have been prepared to support the application and identify any artifacts whilst the enabling works are being undertaken.

15.2 The application has been reviewed in part by Berkshire Archaeology on behalf of the Local Planning Authority and no objection has been raised subject to a condition requiring further investigative works.

## 16.0 **Equalities considerations**

16.1 Throughout this report, due consideration has been given to the potential impacts of development, upon individuals either residing in the development, or visiting the development, or whom are providing services in support of the development. Under the Council's statutory duty of care, the local authority has given due regard for the needs of all individuals including those with protected characteristics as defined in the 2010 Equality Act (e.g.: age (including children and young people), disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation. In particular, regard has been had with regards to the need to meet these three tests:

- Remove or minimise disadvantages suffered by people due to their protected characteristics;
- Take steps to meet the needs of people with certain protected characteristics; and;
- Encourage people with protected characteristics to participate in public life (et al).

16.2 It is considered that there will be temporary (but limited) adverse impacts upon all individuals, with protected characteristics, whilst the proposed works are taking place, by virtue of their nature. It is also considered that noise and dust from the demolition and construction have the potential to cause nuisances to people sensitive to noise or dust. However, conditions are imposed to mitigate such impacts and measures under other legislation covering environmental health will be exercised as and when required.

16.3 In conclusion, it is considered that the needs of individuals with protected characteristics have been fully considered by the Local Planning Authority exercising its public duty of care, in accordance with the 2010 Equality Act.

## 17.0 **Conclusion**

17.1 This enabling works application will facilitate the completion of the demolition and decontamination work for this part of the former AkzoNobel paint coating and manufacturing site.

- 17.2 The application has been evaluated against the Development Plan and the NPPF and the Authority has assessed the application against the core planning principles of the NPPF and whether the proposals deliver “sustainable development.” The report identifies that the proposal, subject to addressing the issues set out in the delegation to Planning Manager would comply with all of the relevant policies in the current Development Plan and the relevant parts of the NPPF. The proposal would comply with Development Plan as whole. The application is therefore recommended for to be delegated to the Planning Manager for approval in accordance with the recommendation set out below.

### **PART C: RECOMMENDATION**

#### 18.0 **Recommendation**

- 18.1 Having considered the relevant policies set out above, and all other relevant material considerations it is recommended the application be delegated to the Planning Manager:

A) For approval subject to finalising conditions, informatives, and any minor changes.

#### 19.0 **PART D: CONDITIONS**

##### Time Limit

1. The development hereby permitted shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

##### Approved Plans

2. The development hereby approved shall be implemented only in accordance with the following plans and information hereby approved by the Local Planning Authority.
  - a) Site Location Plan 22058-BWT-ZZ-XX-DR-A-2000 Rev. P01
  - b) Existing Site Sections 22058-BWT-ZZ-XX-DR-A-2002 Rev. P01
  - c) Proposed Site Sections 22058-BWT-ZZ-XX-DR-A-2202 Rev. P03
  - d) Existing Topographical Survey Sheet 1 of 3 22058-BWT-ZZ-XX-DR-X-0001 Rev. P01
  - e) Existing Topographical Survey Sheet 2 of 3 22058-BWT-ZZ-XX-DR-X-0002 Rev. P01
  - f) Existing Topographical Survey Sheet 3 of 3 22058-BWT-ZZ-XX-DR-



- X-0003 Rev. P01
- g) Demolition Scope Plan 22058-BWT-ZZ-XX-DR-A-2210 Rev. P02
  - h) Proposed Site Levels and Layout Plan 22058-BWT-ZZ-XX-DR-A-2211
  - i) Sheet Pile Retaining Wall Structural Report 22058-BWT-ZZ-XX-RP-S-0006 P05
  - j) Proposed Retaining Wall GA 22058-BWT-ZZ-XX-SK-S-5006 P04
  - k) Proposed Retaining Wall Elevation and Sections 22058-BWT-ZZ-XX-SK-S-5007 P05, 5014 P01 Proposed Retaining Wall – Elevation facing Wexham Road; 5010 P03 Neighbouring boundaries layout; 5011 P03 Neighbouring Cross section (Railway bridge); 5012 P02; Neighbouring Cross section (mid point along boundary); 5013 P02 Neighbour Cross section (North by pedestrian crossing)
  - l) Enabling Works Tree Protection Plan 22058-BWT-ZZ-XX-DR-X-0005 Rev. P014
  - m) Existing Building Elevation Record 22058-BWT-ZZ-ZZ-RP-Z-0001 Rev. P03
  - n) Demolition Scope of Works 22058-BWT-GN-ZZ-SP-S0001 Rev. P08
  - o) Underpass Remedial Works Report 22058-BWT-ZZ-XX-RP-S-0005 Rev. P02
  - p) Proposed Remediation Sequence at Underpass Ramp Plan 22058-BWT-ZZ-XX-SK S-5001 Rev. P02
  - q) Proposed Retaining Wall at Underpass Ramp 22058-BWT-ZZ-XX-SK-S-5002-P02
  - r) Phase 1 Desk Study 22058-BWT-ZZ-XX-RP-X-0002 Rev. P04
  - s) Interim Remediation Strategy 22058-BWT-ZZ-XX-RP-X-0003 Rev. P02
  - t) Remediation Strategy 22058-BWT-ZZ-RP-X-0012 Rev. P02
  - u) Arboricultural Impact Appraisal and Method Statement (Enabling Works)  
prepared by Barrel Tree Consultancy 22058-BWT-ZZ-XX-ROP-X-0006 Rev.P01)
  - v) Manual for Managing Trees on Development Sites 22058-BWT-ZZ-XX-RP-X-0007 Rev. P01
  - w) Demolition Method Statement – including appendix A, B, C and D prepared by ARUP 274921-RP-PM-AKZO-012 Rev. 012
  - x) Preliminary Ecological Appraisal – Equinix Slough Dated 14 Feb 2023 Phase 1 Habitat Survey Dated 24 August 2023
  - y) Preliminary Appraisal Bat Hibernation Survey Report – Equinix Slough Dated 29 March 2023
  - z) Bat Emergence Survey Document – Equinix Slough Dated 19 May 2023
  - aa) Badger Survey Letter Report – Equinix Slough Dated 25 May 2023
  - bb) Preliminary Biodiversity Metric Assessment Technical Note Dated 24 May 2023
  - cc) Cultural Heritage Desk Based Assessment Dated 1 June 2023
  - dd) Detailed Unexploded Ordnance (UXO) Threat & Risk Assessment V.1 Dated 16 June 2023

- ee) Written Scheme of Investigation for geoarchaeological watching brief and trial trench evaluation 09 August 2023 (submitted by email on 29 August)
- ff) Phase II Geo-Environmental Assessment Report Addendum submitted on 1 August 2023 22058-BWT-ZZ-XX-RP-X-0011 P01 (in 4 parts) submitted by email on the 21 July and 1 August 2023
- gg) Flood Risk Assessment and Drainage Strategy 22058-BWT-C1-XX-RP-C-4001 P03 Part 1 to Part 10 (inclusive)
- hh) Proposed Cut and Fill Plan Overview 22058-BWT-C1-00-DR-C-4410 P04

REASON: To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the Policies in the Development Plan.

#### Demolition and Construction Environment Management Plan

2. No demolition or construction shall commence on site until a Demolition And Construction Environment Management Plan to control the environmental effects of demolition and construction has been submitted to and approved in writing by the Local Planning Authority. The Plan shall include the provision to be made to accommodate:
  - i) All site operatives and visitors
  - ii) Construction vehicles which meet a minimum Euro 6/VI Standard
  - iii) Site management of construction traffic and access/haul routes and controlled hours of delivering including;
    - Specification of haul route(s) and of any temporary signage to be provided to identify the route and promote its safe use.
    - Loading and unloading of material.
    - Identification of the routing strategy and procedures for the notification and conveyance of any abnormal or indivisible load authorised by the Highways Agency pursuant to the Road Vehicles (Authorisation of Special Types) (General) Order 2003.
    - Wheel washing facilities and arrangements for removal of mud from public highway.
    - Storage of plant and materials to be used.
    - details of how all temporary external lighting will not result in unacceptable neighbour amenity impacts or ecological impacts.
    - construction working hours, hours during the construction phase, when delivery vehicles taking materials are allowed to enter or leave the site.
  - iv) Measures to be taken in relation to protecting ecology including trees.
  - v) Measures to be taken in relation to the canal to ensure the protection water quality and biodiversity of the waterway, as well as protection of its users.

- vi) Non-road mobile machinery (NRMM) used on site is required to meet Stage IV of EU directive 97/68/EC as a minimum. Machines with constant speed engines (such as generators) are required to meet Stage V of EU directive 97/68/EC.
- vii) The Plan shall also include details of:
  - (i) control of noise, including monitoring against trigger levels
  - (ii) control of dust, including monitoring against trigger levels

The Plan shall thereafter be implemented as approved before development begins and throughout the duration of the demolition and construction works period.

REASON: In the interest of minimising danger and inconvenience to highway users/residents and in the interests of air quality in accordance with Core Policy 7 and 8 of the Adopted Local Development Framework, Core Strategy 2006 - 2026, and the requirements of the National Planning Policy Framework 2023.

#### Dust Management Plan

3. No demolition shall commence on site until a Dust Management Plan to control dust emissions during the demolition phase has been submitted to and approved in writing by the Local Planning Authority. The plan shall include details of dust control measures and monitoring, which aligns with IAQM 'Guidance on the assessment of dust from demolition and construction'.

The Plan shall thereafter be implemented as approved before development begins and throughout the duration of the demolition works period.

REASON: In the interest of danger and inconvenience to highway users/residents and in the interests of air quality in accordance with Core Policy 7 and 8 of the Adopted Local Development Framework, Core Strategy 2006 - 2026, and the requirements of the National Planning Policy Framework 2023.

#### Remediation Validation

4. No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Akzo Nobel DC – Remediation Strategy, Ref. no. 2731-A2S-XX-XX-RP-Y-0005-02, dated August 2023, and prepared by A-squared Studio Ltd shall be occupied until a full final Validation Report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority.

The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Remediation

Strategy above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation that all such measures have been implemented by a competent installer and then verified by a qualified independent third party/Building Control Regulator.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health and in accordance with Policy 8 of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2023.

## Drainage

5. Before any above ground works commence a detailed design of surface water drainage scheme for the site based on sustainable drainage principles and an assessment of the hydrological and hydro geological context of the development should be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed. The scheme shall include:
  - i) Details of the drainage system are to be accompanied by full and appropriately cross-referenced supporting calculations which will include a 10% allowance for urban creep.
  - ii) Cross sections of the control chambers (including site specific levels mAOD) and manufacturers' hydraulic curves should be submitted for all hydrobrakes and other flow control devices.
  - iii) Detailed scheme for the ownership and scheduled maintenance for every element of the surface water drainage system.
  - iv) Confirmation of site-specific soil conditions to confirm or exclude use of infiltration solutions.

REASON: To reduce the risk of flooding both on and off site in accordance with the NPPF and Policy 5 of the Core Strategy for North Northamptonshire by ensuring the satisfactory means of surface water attenuation and discharge from the site and to ensure the future maintenance of drainage systems associated with the development.

6. No development shall take place until a detailed scheme for the ownership and maintenance for every element of the surface water drainage system proposed on the site has been submitted to and approved in writing by the Local Planning Authority and the maintenance plan shall be carried out in full thereafter. The scheme should include:
  - i) Details are required of which organisation or body will be the main maintaining body where the area is multifunctional (e.g., open space play areas containing SuDS) with evidence that the organisation/body

- has agreed to such adoption.
- ii) A maintenance schedule setting out which assets need to be maintained, at what intervals and what method is to be used.
- iii) A site plan including access points, maintenance access easements and outfalls. Maintenance operational areas to be identified and shown on the plans, to ensure there is room to gain access to the asset, maintain it with appropriate plant and then handle any arisings generated from the site.
- iv) Details of expected design life of all assets with a schedule of when replacement assets may be required.

REASON: To ensure the future maintenance of drainage systems associated with the development.

7. No occupation shall take place until the Verification Report for the installed surface water drainage system for the site based on the approved Flood Risk Assessment & Sustainable Drainage Strategy, ref P/00071/128 FLOOD RISK ASSESSMENT AND DRAINAGE STRATEGY has been submitted in writing by a suitably qualified drainage engineer and approved by the Local Planning Authority. The report shall include:

- i) How any departure from the agreed design is in keeping with the approved principles.
- ii) Any As-Built Drawings and accompanying photos.
- iii) Results of any Performance testing undertaken as a part of the application process (if required / necessary)
- iv) Copies of any Statutory Approvals, such as Land Drainage Consent for Discharges etc.
- v) CCTV confirmation that the surface water drainage system is free from defects, damage, and foreign objects.
- vi) Confirmation of adoption or maintenance agreement for all SuDS elements as detailed within the drainage strategy is in place.

REASON: To ensure the installed Surface Water Drainage System is satisfactory and in accordance with the approved reports for the development site.

Environment Agency

8. If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the local planning authority) shall be carried out until a remediation strategy detailing how this contamination will be dealt with has been submitted to, and approved in writing by, the local planning authority. The remediation strategy shall be implemented as approved.

REASON: To ensure that the development does not contribute to, and is not put at unacceptable risk from or adversely affected by, unacceptable levels of

water pollution from previously unidentified contamination sources at the development site. This is in line with paragraph 174 of the National Planning Policy Framework.

9. Prior to any part of the permitted development being brought into use, a verification report demonstrating the completion of works set out in the approved remediation strategy and the effectiveness of the remediation shall be submitted to, and approved in writing, by the local planning authority. The report shall include results of sampling and monitoring carried out in accordance with the approved verification plan to demonstrate that the site remediation criteria have been met.

REASON: To ensure that the site does not pose any further risk to the water environment by demonstrating that the requirements of the approved verification plan have been met and that remediation of the site is complete. This is in line with paragraph 174 of the National Planning Policy Framework.

10. No drainage systems for the infiltration of surface water to the ground are permitted other than with the written consent of the local planning authority. Any proposals for such systems must be supported by an assessment of the risks to controlled waters. The development shall be carried out in accordance with the approved details.

REASON: To ensure that the development does not contribute to and is not put at unacceptable risk from or adversely affected by, unacceptable levels of water pollution caused by mobilised contaminants. This is in line with paragraph 174 of the National Planning Policy Framework.

11. Piling using penetrative methods shall not be carried out other than with the written consent of the local planning authority. The development shall be carried out in accordance with the approved details.

REASON: To ensure that the proposed re-development, does not harm groundwater resources in line with paragraph 170 of the National Planning Policy Framework and Position Statement N of the 'The Environment Agency's approach to groundwater protection'. Piling using penetrative methods can result in risks to potable supplies from, for example, pollution/turbidity, risk of mobilising contamination, drilling through different aquifers and creating preferential pathways.

12. A scheme for managing any borehole installed for the investigation of soils, groundwater or geotechnical purposes shall be submitted to and approved in writing by the local planning authority. The scheme shall provide details of how redundant boreholes are to be decommissioned and how any boreholes that need to be retained, post-development, for monitoring purposes will be secured, protected and inspected. The scheme as approved shall be implemented prior to the occupation of any part of the permitted development.

REASON: To ensure that redundant boreholes are safe and secure, and do not cause groundwater pollution or loss of water supplies in line with paragraph 174 of the National Planning Policy Framework and Position Statement A of 'The Environment Agency's approach to groundwater protection'.

#### Network Rail

13. Prior to the construction of the Wrexham Road retaining wall (as shown on drawings 22058-BWT-ZZ-XX-SK-S-5006 P03 and 22058-BWT-ZZ-XX-SK-S-5007 P04), a detailed design and method statement indicating the piling details shall be submitted to and approved in writing by the Local Planning Authority. The development shall thereafter be carried out in all respects in accordance with the approved details.

REASON: To safeguard adjoining railway infrastructure.

#### Archaeology

14. A) No development shall take place/commence until a programme of archaeological work including a Written Scheme of Investigation (WSI) has been submitted to and approved in writing by the Local Planning Authority. The WSI shall include an assessment of significance and research questions; and:

- i) The programme and methodology of site investigation and recording.
- ii) The programme for post investigation assessment.
- iii) Provision to be made for analysis of the site investigation and recording.
- iv) Provision to be made for publication and dissemination of the analysis and records of the site investigation.
- v) Provision to be made for archive deposition of the analysis and records of the site investigation.
- vi) Nomination of a competent person or persons/organisation to undertake the works set out within the WSI.

B) The Development shall take place in accordance with the WSI approved under condition A).

The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the WSI approved under condition A) and the provision made for analysis, publication and dissemination of results and archive deposition has been secured.

REASON: The site lies in an area of archaeological potential, particularly for,

but not limited to, Prehistoric and Medieval remains. The potential impacts of the development can be mitigated through a programme of archaeological work. This is in accordance with national and local plan policy.

#### Construction plant noise

15. Prior to the commencement of any construction works, full details of construction plant noise levels and proposed mitigation consistent with the noise criteria from BS5228 shall be submitted to and approved in writing by the Local Planning Authority. The construction phase shall be carried out in full accordance with these details.

REASON: To ensure that the development hereby permitted is not detrimental to the amenity of the surrounding area by reason of undue noise emission and/or unacceptable disturbance. In accordance with Policy EN1 and EMP2 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the National Planning Policy Framework 2023.

#### Retaining wall

16. Prior to the erection of the retaining wall along Wexham Road, details of its construction, design and brickwork finish shall be submitted to and approved in writing by the Local Planning Authority. The retaining wall shall then be completed in full accordance with the approved details.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Core Policies 7 and 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008, Policies EN1 and EMP2 of the Adopted Local Plan for Slough 2004, and the requirements of the National Planning Policy Framework 2023.

#### Informatives:

1. As the proposed development is within the Consultation Distance of a major hazard pipeline you should consider contacting the pipeline operator before deciding the case. There are two particular reasons for this:
  - The operator may have a legal interest (easement, wayleave etc.) in the vicinity of the pipeline. This may restrict certain developments within a certain proximity of the pipeline;
  - The standards to which the pipeline is designed and operated may restrict occupied buildings or major traffic routes within certain proximity of the pipeline. Consequently, there may be a need for the operator to modify the pipeline, or its operation, if the development proceeds.



HSE's website provides advice on a wide range of topics, including the fire and explosion risks associated with flammable substances – see About dangerous substances - Fire and explosion ([hse.gov.uk](http://hse.gov.uk)) and Storage of flammable liquids in tanks HSG176 ([hse.gov.uk](http://hse.gov.uk)).

The employer will be subject to the requirements of the Health and Safety at Work etc Act 1974 (HSWA) and associated legislation, including The Dangerous Substances And Explosives Atmospheres Regulations 2002 – see The Dangerous Substances and Explosive Atmospheres Regulations 2002 - Fire and explosion ([hse.gov.uk](http://hse.gov.uk)).

Under sections 2 and 3 of the HSWA, an operator must conduct the undertaking in such a way as to ensure that, so far as is reasonably practicable, employees and other persons, including people living nearby, are not thereby exposed to risks to their health or safety.

2. The applicant is advised to contact Network Rail Asset Protection ([assetprotectionwestern@networkrail.co.uk](mailto:assetprotectionwestern@networkrail.co.uk)) when details relating to condition 13 are provided.

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Registration Date:	31-May-2023	Application No:	P/00740/008
Officer:	Michael Scott	Ward:	Slough Central
Applicant:	TR Land Acquisition Ltd	Application Type:	Major
		13 Week Date:	30 August 2023 [EoT 30 Nov 2023]
Agent:	Simon Millett, Walsingham Planning, Bourne House, Cores End Road, Bourne End, SL8 5AR		
Location:	ICI Dulux Decorator Centre, Petersfield Avenue, Slough, Berkshire, SL2 5EA		
Proposal:	Construction of a five storey building with basement level to provide 22 no. residential apartments with associated access, parking, landscaping and all associated works.		

**Recommendation:** Delegate to the Planning Manager



## 1.0 **SUMMARY OF RECOMMENDATION**

1.1 Having considered the relevant policies of the Development Plan set out below, and representations that have been received from consultees and the community, and all other relevant material considerations, it is recommended the application be delegated to the Planning Manager:

A) Approval subject to:

- (i) The satisfactory completion of a Section 106 Agreement to secure affordable housing, education and Burnham Beeches contributions, funding towards a zebra crossing (via a Section 278 agreement) that is required to mitigate the impact of the development.
- (ii) Finalising conditions and any other minor changes;

or

B) Refuse the application if the completion of the Section 106 Agreement is not finalised by 31 May 2024 unless a longer period is agreed by the Planning Manager, or Chair of the Planning Committee.

1.2 Under the current constitution, this application is to be determined at Planning Committee, as it is an application for a major development comprising more than 10 dwellings.

## **PART A: BACKGROUND**

### 2.0 **Proposal**

2.1 This is a full planning application for:

- Demolition of the existing commercial unit.
- Construction of a part four/part five-storey building.
- Provision of 22 self-contained residential units with a mix of:
  - 6no. 3-bedroom units;
  - 11no. 2-bedroom units and
  - 5no. 1-bedroom units.
- The formation of a semi-basement (lower ground floor) level to provide on-site parking for cars and a van, as well as, and cycle storage facilities.
- Access to the lower ground floor level parking court would be from a ramp at the south-west corner of the site on Petersfield Avenue.

- The car parking would comprise 22 spaces with one space designated for blue badge holder.
- Each of the 22 spaces would be provided with an EV charging facility.
- The provision of an on-site space for a van enabling deliveries off-street at lower ground floor level.
- Secure bin and recycling storage facilities at ground floor level accessed from the vehicular ramp.
- Roof level provision of solar panels.

### 3.0 **Application Site**

- 3.1 The site is that of the former Dulux Decorator Centre which still comprises a detached, single-storey, rectangular building that sits back from the front of the site with hardstanding around. The building was used for the sales of paint and other decorators' equipment under Class E. The building is now vacated with some use of the hardstanding by the adjacent car service centre. Access points exist at the south-western corner of the site and the south-eastern corner, too.
- 3.2 Ground levels at the front of the application site are similar between it and the neighbouring adjacent sites on Petersfield Avenue with little difference to that behind at the properties in St Paul's Avenue.
- 3.3 To the south, almost directly opposite the current application site lies the distinctive three limbed office building on the main Akzo Nobel site. This large comprises the offices closest to the Wexham Road frontage and a vast area of open parking in two parts either side of an access and circulation roadway, somewhat to the south-west of the application site.
- 3.4 To the west is the Slough Tyre centre premises. This comprises an equivalent of a two-storey building with "blind" (i.e. it has no window openings), metal clad exterior. There is a large open hardstanding at the south-east corner of the site adjacent to the current application site. The vehicular access to the premises is at the back of the hardstanding.
- 3.5 To the east is the site occupied by Kia Motors. Their premises comprise a split level building of equivalent two-storeys, with the higher part on the rising ground levels approaching Wexham Road to the east. There is an extensive area of forecourt parking for car sales.
- 3.6 To the north lie residential properties on St Paul's Avenue. These are two-storey, semi-detached dwellings with long south facing gardens, many of which have detached outbuildings at their southern end adjacent to the application site boundary but at a slightly higher ground level.

- 3.7 There are currently two access points from Petersfield Avenue – one to the south-west and the other to the south-east of the frontage.
- 3.8 The site lies at a location designated as a designated Business Area in the Local Plan and falls within Flood Zone 1.
- 3.9 For completeness, it should be noted: The site is located outside of the town centre; the site does not lie in a conservation area; there are no heritage assets nearby; and there are no protected trees in the vicinity.

#### 4.0 **Site History**

- 4.1 There is no planning history for the site.
- 4.2 A pre-application enquiry – reference. 1553 – was described by the agent in their form as development by “Five-storey building to provide 22 flats.”

This involved the re-development of the current application site by a five storeys building of 22 residential units having a mix of units consisting of 5 x 1 bed, 11 x 2 bed and 6 x 3 bed flats.

- 4.3 The officers’ concluding remarks in a letter dated 1<sup>st</sup> December 2022 regarding Pre-App/1553 were, as follows:

*“It is considered that the principle of development could be found to be acceptable subject to a more robust approach to justifying the land use proposed.*

*Other issues that have been raised can be addressed through design which may materially affect the proposal however if the loss of employment land can be justified, designs could progress to address the impacts.”*

- 4.4 Of relevance to the assessment of the current application is the recent history of nearby sites; namely:

BMW House (P/00988/015) - Demolition of the existing B8 and B1 office and warehouse and the construction of a part 4, part 3 and part 2 no. storey residential building comprising of 24 no apartments, with a semi basement car park - APPROVED – 14/03/2018 – Now built.

BMW House extension (P/00988/016) - Construction of an additional storey on top of existing apartment building. New floor to provide 4no. additional apartments comprising 3no. 1 bed apartments and 1no. 2 bed apartment, and associated parking – APPROVED – 02/05/2019 – Now built.

Akzo Nobel (P/00072/096) - Outline planning application ... for mixed use development comprising:... up to 1,000 residential dwellings; along with flexible commercial uses including ... use classes A1 (Shops), A2 (Financial and Professional Services), A3 (Food and Drink), D1 (Non-residential Institutions) and D2 (Assembly and Leisure); ...and the provision of commercial floorspace ... use classes B2 (General Industry), B8 (Storage or Distribution) and sui generis data centre – APPROVED – 19/11/2020. – Now under construction.

Thomas House (P/02028/008) - Demolition of the existing building (Use Class B2) and construction of a 5 storey building with lower ground parking, to accommodate 18 residential units (Use Class C3) with associated works – Resolution to grant permission, subject to a section106 Agreement.

Grace House (P/19642/000) - Demolition of the existing B8 commercial building, and the construction of a 5 storey building, with semi-basement parking to provide 50 no. residential apartments – Resolution to grant permission, subject to a section 106 Agreement.

## 5.0 **Neighbour Notification**

5.1 In accordance with Article 15 of The Town and Country Planning (Development Management Procedure, Listed Buildings and Environmental Impact Assessment) (England) (Coronavirus) (Amendment) Regulations 2020, four site notices were displayed relating to the current submission – two on lampposts in the immediate vicinity of the site’s location on Petersfield Avenue and two on lampposts in the vicinity of the rear of the site on St Paul’s Avenue – each respectively on 24/07/2023. The application was advertised as a major application in the 14/07/2023 edition of The Slough Express.

5.2 No representations have been received.

## 6.0 **Consultations**

### 6.1 **Highway Authority**

#### **Introduction**

This document provides Slough Borough Council’s consultation response regarding Highways and Transport for planning application No. P/00740/008 at the ICI Dulux Decorators Centre on Petersfield Avenue, SL2 5EB.

#### **Vehicle Access**

SBC Transport Officers would have no objection to the proposed vehicle access arrangements for the proposed development. The NPPF outlines in Paragraphs 110 and 112 that developments provide safe and suitable access for all users and that in assessing applications for development it should be ensured that: *‘The design of streets, parking areas, other transport elements and the content of associated standards reflects current national guidance, including the National Design Guide’.*

The applicant has demonstrated that suitable visibility can be provided in accordance with Manual for Streets at the proposed vehicle access junction. RGP Drawing No. 2023/7046/003-Rev-P1 titled *‘Visibility Splays’* shows 2.4m x 43m of visibility in accordance with the Manual for Streets visibility standard for a 30mph speed limit.

The redundant crossover would be closed, and the footway/kerb reinstated across the eastern crossover by the developer. Swept path analysis has been provided which demonstrates that there is suitable space within the development for a 4.572m long car to turn within the site and a 3.5T/5.339m long panel van.

The applicant will be required to enter into a Section 278 agreement for the access works in the event that the application is granted approval.

#### Zebra Crossing Section 106 Contribution

The applicant has agreed a contribution of £37,097 towards the provision of a Zebra Crossing on Petersfield Avenue. This will be secured by Section 106 agreement. This will allow the council to provide a crossing facility and facilitate additional demand for residents to walk towards Slough Railway Station, Tesco Extra and Slough Town Centre.

#### Car Parking

SBC Transport Officers are satisfied with the number of car parking spaces proposed on site. 22 car parking spaces are proposed on site, providing 1 car parking space per dwelling.

Slough’s Car Parking Standards dwellings in the ‘Rest of Town Centre’ area require 40 car parking spaces. However, SBC Transport Officers are satisfied with 1 space per dwelling given Slough Railway Station, Slough Town Centre and Tesco Extra are situated within walking distance of the site.

<b>Slough Parking Standards (Rest of Town Centre)</b>		
<b>Dwelling Type</b>	<b>Spaces Per Dwelling</b>	<b>Car Parking Requirement</b>
1 Bedroom flat (x5)	1.5	8
2 Bedroom flat (x11)	1.75	20



3 Bedroom flat (x6)	2	12
<b>Total Car Parking Spaces</b>		<b>40</b>

Slough's Car Parking standards define Petersfield Avenue as an Existing Business Area. The standards for the Rest of Town Centre area have been applied by officers given Petersfield Avenue has become more residential during the past decade with light industrial sites being redeveloped.

The number of car parking spaces proposed is also consistent with the car parking provided at other housing developments approved by SBC on Petersfield Avenue such as BMW House (P/00988/015: 24 dwellings and 24 car parking spaces) and Thomas House (P/02028/008: 18 dwellings and 18 car parking spaces).

#### Disabled Car Parking

One disabled car parking space is proposed on site. This meets DfT requirements for 5% of car parking spaces to be set aside for Blue Badge/Disabled drivers (Inclusive Mobility, DfT, 2021).

#### Electric Vehicle Parking

SBC are satisfied with the proposal to provide 1 EV Charging Point per dwelling. The Slough Low Emissions Strategy (2018 – 2025) requires the provision of 1 EV Charging Point per dwelling, for dwellings with allocated parking.

The National Planning Policy Framework Paragraph 112 requires applications for development to: *'Be designed to enable charging of plug-in and other ultra-low emission vehicles in safe, accessible, and convenient locations'*.

#### Deliveries, Servicing and Refuse Collection

SBC Transport Officers have no objection to the development due to the arrangements for managing deliveries. The majority of delivery vehicles will stop on Petersfield Avenue as with the arrangements for accommodating deliveries to the flats at BMW House and other houses/flats along Petersfield Avenue and Mill Street.

A parking space for a small van has been provided which could accommodate any small vans that need to enter the site for scenarios such as removals.

NPPF Paragraph 112 requires that applications for development *'allow for the efficient delivery of goods, and access by service and emergency vehicles'*.

### Cycle Parking

SBC Transport officers require further details of cycle parking are secured by planning condition. 22 cycle parking spaces should be provided. The proposed site plan displays cycle parking in the form of individual cycle lockers; although some lockers appear too small and the 22 spaces would be more efficiently provided in an open plan store.

The Slough Developers' Guide – Part 3: Highways and Transport (2008) requires the provision of 1 secure and covered cycle parking space per dwelling to encourage the uptake of cycling within the borough.

### Summary and Conclusions

I can confirm that I have no objection to the proposed development on highways and transport grounds. I would recommend that any approval is subject to the conditions and informatives [as set out in the Recommendation at 26.0] below.

#### 6.2 Thames Water: *Waste Comments*

The proposed development is located within 15 metres of a strategic sewer. Thames Water requests a piling method statement condition to be added to any planning permission and informatives.

With regard to surface water drainage, Thames Water would advise that if the developer follows the sequential approach to the disposal of surface water we would have no objection, an informative has been included.

Thames Water would advise that with regard to waste water network and sewage treatment works infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

#### *Water Comments*

On the basis of information provided, Thames Water would advise that with regard to water network and water treatment infrastructure capacity, we would not have any objection to the above planning application. Thames Water recommends an informative be attached to this planning permission.

### 6.3 Lead Local Flood Authority

We would advise that there is sufficient information available to comment on the acceptability of the proposed surface water drainage scheme for the proposed development.

We consider that if the scheme is recommended for approval that planning conditions are included as the impacts of surface water drainage will have been adequately addressed at this stage. Without these conditions, the proposed development on this site may pose an unacceptable risk of flooding.

### 6.4 SBC Scientific Officer

I have reviewed the Phase I Contamination Assessment (“Desk Study”), reference no: LS6724, dated 11<sup>th</sup> April 2023, and prepared by Land Science.

Please see my comments below:

- According to the Council’s Priority List the proposed development is located on a site identified as low risk.
- The Conceptual Site Model (CSM) and Preliminary Risk Assessment (PRA) identified potential sources of contamination and recommend that further site intrusive investigation of carried out in order to assess and mitigate any potential residual risk to the proposed human health receptors.
- The report is approved.

Based on the above I recommend the conditions [as set out in the Recommendation at 26.0 below]

### 6.5 SBC Environmental Quality Officer

- The environmental noise assessment has been prepared by Scotch Partners in support of this application. The assessment is informed by a noise survey conducted from 28<sup>th</sup> – 30<sup>th</sup> March 2023, and additional measurements taken of lorry activities to assess the impact of HGVs passing the site from the AkzoNobel trans-shipping development and other commercial developments on Petersfield Avenue. The noise generated by the adjacent tyre fitting depot has also been considered.
- The assessment has endeavoured to follow a worst-case scenario by assessing the impact of 16 vehicle trips during the night (when the condition attached to AkzoNobel restricts movements to 10 per night), on the worst affected room (corner bedroom on front

- elevation with full glazing), using the full range of noise frequencies rather than providing an average. This approach is supported.
- With the application of glazing and ventilation specifications outlined in Section 5.3, it is expected that internal noise levels can be achieved. Due to the high external noise levels however, the internal noise levels will not be achieved with open windows. As such, the report recommends acoustic trickle ventilation, however it is stated in the report that other glazing and ventilation configurations may be suitable, and that the precise window specifications and their sound insulation performance would need to be determined at the detailed design stage. As such, details of this will be required via condition, which evidence that the choosing glazing and ventilation meet the recommendations outlined in the noise report, alongside manufacturer details and a schematic drawing indicating locations of specific glazing types and ventilation installations.
  - The report has not considered noise impacts on external amenity spaces (balconies). It is shown in the design drawings that balconies will be facing Petersfield Avenue, which is expected to have the highest noise levels. Based on the external noise measurements, it is expected that the noise levels on the front façade will exceed the external noise level criteria of 55dB LAeq by approximately 7dB, with frequent HGV pass-by events, therefore it is unlikely that future occupants will be able to enjoy this space. It has been argued with previous applications however that future occupants would prefer to have access to this space despite the noise, than to not have any external amenity at all, therefore it should be a planning matter as to whether the balconies are retained.
  - Although the noise report has commented on construction noise, an assessment has not been undertaken, due to the lack of detail on construction methodologies. It is requested therefore that a CEMP is provided via condition, which includes details of noise mitigation.

## 6.6 Natural England

Objection - further information required to determine impacts on designated sites - development within 5.6 kilometres of Burnham Beeches Special Area of Conservation (SAC) within 5.6 kilometres

It is Natural England's view that the planning authority will not be able to ascertain that this proposed development as it is currently submitted would not adversely affect the integrity of the SAC. In combination with other plans and projects, the development would be likely to contribute to a deterioration of the quality of the habitat by reason of increased access to the site including access for general recreation and dog-walking. There being alternative solutions to the proposal and there being no imperative reasons of overriding public interest to allow the

proposal, despite a negative assessment, the proposal will not pass the tests of Regulation 62.

This application is supported by a HRA screening report (May 2023). Natural England agree with the conclusion that the proposed development will have a Likely Significant Effect on Burnham Beeches SAC and therefore mitigation measures will be necessary to rule out adverse effects on integrity.

Upton Court Park Suitable Alternative Natural Greenspace (SANG) could function as the mitigation for this development with contributions made towards its improvement, should the SANG have sufficient capacity remaining.

## **PART B: PLANNING APPRAISAL**

### **7.0 Policy Background**

#### **7.1 National Planning Policy Framework (2023) and National Planning Policy Guidance:**

Section 2: Achieving sustainable development

Section 4: Decision-making

Section 5: Delivering a sufficient supply of homes

Section 8: Promoting healthy communities

Section 9: Promoting sustainable transport

Section 11: Making effective use of land

Section 12: Achieving well-designed places

Section 14: Meeting the challenge of climate change, flooding and coastal change

#### **7.2 The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008**

Core Policy 1 – Spatial Strategy

Core Policy 3 – Housing Distribution

Core Policy 4 – Type of Housing

Core Policy 7 - Transport

Core Policy 8 – Sustainability and the Environment

Core Policy 9 – Natural, built and historic environment

Core Policy 10 – Infrastructure

Core Policy 11 - Social cohesiveness

Core Policy 12 – Community Safety

#### **7.3 The Adopted Local Plan for Slough 2004 (Saved Policies)**

EN1 – Standard of Design

EN3 – Landscaping Requirements

EN5 – Design and Crime Prevention

EMP3 – Loss of Employment Land

H9 – Comprehensive Planning  
H14 – Amenity Space  
S1 – Retail Hierarchy  
T2 – Parking Restraint  
T8 – Cycle Network and Facilities  
T9 – Bus Network and Facilities  
OSC15 – Provision of Facilities in new Residential Developments

#### Other Relevant Documents/Statements

- Slough Borough Council Developer's Guide Parts 1-4
- Slough Local Development Framework Proposals Map (2010)
- Technical Housing Standards – nationally described space standards.
- ProPG: Planning & Noise: Professional Practice Guidance on Planning & Noise. New Residential Development. May 2017

#### 7.4 The Proposed Spatial Strategy (Nov 2020)

Under Regulation 18, the Proposed Spatial Strategy for the Local Plan for Slough was the subject of public consultation in November 2020. This set out a vision and objectives along with proposals for what the pattern, scale and quality of development will be in Slough. The consultation document contained a revised Local Plan Vision which supports the Council's vision for Slough as a place where people want to "work, rest, play and stay."

It should be noted that the consultation document for the Proposed Spatial Strategy does not contain any specific planning policies or allocate any sites. It made it clear that the existing planning policy framework for Slough would remain in force until replaced by new Local Plan policies in the future. Nevertheless, it sets out the most up to date statement of the Council's position with regards to strategic planning issues. As a result, it is relevant for the consideration of this application (but only very limited weight can be afforded to the specific and strategic guidance therein).

#### 7.5 Habitats Regulations Assessment of Projects, Natura 2000 and European Sites

Natura 2000 is the cornerstone of European nature conservation policy; it is an EU-wide network of Special Protection Areas (SPA) classified under the 1979 Birds Directive and Special Areas of Conservation (SAC) designated under the 1992 Habitats Directive. Since 31st December 2020, the UK requirements for Habitat Regulations Assessments is set out in the Conservation of Habitats and Species Regulations 2017 (as amended by the Conservation of Habitats and Species Amendment (EU Exit) Regulations 2019). Together, the National Site Network of the UK comprises over 25,500 sites and safeguards the most valuable and threatened

habitats and species across Europe and the UK; it represents the largest, coordinated network of protected areas in the world.

HRA employs the precautionary principle and Reg. 102 ensures that where a project is 'likely to have a significant effect' (LSE), it can only be approved if it can be ascertained that it 'will not adversely affect the integrity of the European site'. Burnham Beeches is designated a SAC under this Directive which is located to the north of Slough. The development 'project' has been screened (as part of the Habitat Regulations Assessment) and it has been identified that LSE cannot be ruled out at this stage. An Appropriate Assessment is therefore required to determine whether mitigation measures are required to ensure the project will not adversely affect the integrity of the European Site (Burnham Beeches SAC).

7.6 Buckinghamshire SPD Burnham Beeches Special Area of Conservation

Buckinghamshire Council adopted (in November 2020) a Supplementary Planning Document (Burnham Beeches Special Area of Conservation – strategic Access Management and Monitoring Strategy) which requires developers to make a financial contribution per dwelling for mitigation irrespective of dwelling type or size in a zone between 0.5km and 5.6km from Burnham Beeches. The threshold, in terms of the size of development, when a contribution will apply is for schemes of 10 net additional homes.

7.7 Slough Local Development Plan and the NPPF

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given). The revised version of the National Planning Policy Framework (NPPF) was published in September 2023.

The National Planning Policy Framework 2023 states that decision-makers at every level should seek to approve applications for sustainable development where possible and planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

Following the application of the updated Housing Delivery Test set out in the National Planning Policy Framework 2023, the Local Planning Authority cannot demonstrate a Five-Year Land Supply. Therefore, when applying Development Plan Policies in relation to

the development of new housing, the presumption in favour of sustainable development will be applied, which comprises a tilted balance in favour of the development as set out in Paragraph 11(d) (ii) of the National Planning Policy Framework 2023 and refined in case law. The 'tilted balance' as set out in the NPPF paragraph 11 requires local planning authorities to apply the presumption in favour of sustainable development (in applications which relate to the supply of housing) unless any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

Planning Officers have considered the revised National Planning Policy Framework 2023 which has been used together with other material planning considerations to assess this planning application.

## 7.8 Equality Act

In addition, Section 149 of the Equality Act (2010) which sets a Public Sector Equality Duty (PSED) came into force in April 2011 and requires the Council to consider the equality impacts on all protected groups when exercising its functions. In the case of planning, equalities considerations are factored into the planning process at various stages. The first stage relates to the adoption of planning policies (national, strategic and local) and any relevant supplementary guidance. In coming to a recommendation, officers have considered the equalities impacts on protected groups in the context of the development proposals as set out below in this report.

## 7.8 The planning considerations for this proposal are:

- Principle of development
- Impact on the character and appearance of the area
- Impact on amenity of neighbouring occupiers
- Housing mix
- Living conditions for future occupiers of the development
- Crime prevention
- Highways and parking
- Flooding and drainage
- Trees and landscaping
- Habitats
- Energy and sustainability
- Air quality
- Heritage issues
- Land contamination
- Infrastructure and Section 106 Contributions
- Presumption in favour of sustainable development
- Equalities Considerations



## 8.0 **Principle of development**

- 8.1 The site currently comprises the premises formerly used as a paint/decorators centre, which represents commercial/employment generating floor space, whilst performing a retail function. Therefore, these proposals entail the loss of employment generating floor space – last used as retail (Class E). The existing gross floorspace of the premises comprises approx. 250 sqm.
- 8.2 Paragraph 119 of the NPPF (2023) states that planning policies and decisions should promote an effective use of land in meeting the need for homes and other uses, while safeguarding and improving the environment and ensuring safe and healthy living conditions. Strategic policies should set out a clear strategy for accommodating objectively assessed needs, in a way that makes as much use as possible of previously-developed or 'brownfield' land.
- 8.3 Paragraph 123 of the NPPF (2023) states local planning authorities should also take a positive approach to applications for alternative uses of land which is currently developed but not allocated for a specific purpose in plans, where this would help to meet identified development needs. In particular, they should support proposals to: a) use retail and employment land for homes in areas of high housing demand, provided this would not undermine key economic sectors or sites or the vitality and viability of town centres, and would be compatible with other policies in this Framework.
- 8.4 Paragraph 187 of the NPPF (2023) sets out that new development should be integrated effectively with local businesses and community facilities (such as places of worship, pubs, music venues and sport clubs). Existing businesses and facilities should not have unreasonable restrictions placed on them as a result of development permitted after they were established. The introduction of further residential accommodation in a mixed-use area where there are existing businesses shall be considered below.
- 8.5 Policy 4 of the Core Strategy states that in the urban areas outside the town centre, new residential development will predominantly consist of family housing and be at a density related to the character of the surrounding area, the accessibility of the location, and the availability of existing and proposed local services, facilities and infrastructure.
- 8.6 Given the nature of the former commercial use of the site by ICI Dulux, the starting point in considering a scheme for the redevelopment of the site involving the loss of employment floorspace in terms of the Local Development Plan is Core Policy 5. This sets out that "*There will be no loss of the defined Existing Business Areas to non-employment generating uses, especially where this would reduce the range of jobs available*".

8.7 The proposal would result in the loss of 250 sqm. of employment floorspace, and it would therefore represent an exception to policy, which must be given significant weight in the planning balance.

8.8 The applicant's submission includes a Sales and Marketing Report, which states:

*"When it was known that the previous occupant was to move to new premises, the building was marketed by Sheridan Estates on the open market, including on Rightmove, Onthemarket.com and Auction House. It was also marketed via property portals which target prospective buyers and Sheridan Estate also approached its clients and contacts directly informing them of the availability of the premises. The premises were marketed for a period of 7 months."*

8.9 The report concludes:

*"The premises were marketed for all uses, including retail, commercial and light industrial. No offers were received by these uses during the marketing period, apart from for the temporary/short term use of the forecourt for car washing and car storage at a significantly reduced rate and to keep the site secure and maintained."*

*The lack of permanent interest in the existing building is a result of its age and poor condition. Without major adaptation or its demolition and rebuild, the building is unsuitable and not fit for purpose to meet modern expectations and requirements of businesses and their staff. This is one of the reasons the previous occupier vacated the premises, with more attractive and fit for purpose premises readily available in the local area.*

*The required works to bring the building/site up to standard would render renting or purchasing the building unviable. It is therefore concluded that the building is beyond viable repair and is at the end of its life. There are also plentiful alternative commercial premises available in the local area, such as the Slough Trading Estate."*

8.10 Therefore, it is considered that the loss of employment has to be assessed in relation to the thrust of paragraph 119 of the NPPF, as the site is proposed for residential use, in order to meet the housing allocation.

8.11 As Slough does not currently have a five housing land year supply, paragraph 11 of the NPPF must be considered. This states that in making decisions on applications,

*"d) where ...the policies which are most important for determining the application are out-of-date, granting permission unless:*

- i. the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed; or*
- ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.”*

- 8.12 Whilst further consideration shall be given to the significance of this matter under the tilted balance section below in this report, suffice to say here that the argument in favour of the proposals is met in the NPPF.
- 8.13 Furthermore, the emerging change in the composition of the area given the approval of the scheme for residential development at BMW House, Grace House, Thomas House and the Akzo Nobel site, as identified in paragraph 4.4 above, there is now a compelling case to support the principle of the proposed scheme given the need for housing and changing nature of the area. Overall, it is considered that this site is an opportunity to continue a residential belt on the north side of Petersfield Avenue that would contribute to a streetscene enhancement in this area.
- 8.14 Although there was the pre-existing retail use of the site by ICI, the site does not lie in a Designated Shopping Area, as such the retail function/use is not accorded any protection under the Local Development Plan, notwithstanding this it is acknowledged that jobs would be lost as a result of the proposed development.
- 8.15 In terms of the overarching requirement to effectively and efficiently use land, these proposals involve the redevelopment of the existing commercial floorspace by a more substantial building providing 22 new residential units. As such, it is considered that the proposals comply with the overall thrust of the NPPF.
- 8.16 Core Policies 1 and 4 which seek high-density, non-family type housing to be located in the Town Centre. In the urban areas outside of the town centre, new residential development is expected to be predominantly family housing and be at a density related to the character of the surrounding area, the accessibility of the location, and the availability of existing and proposed local services, facilities and infrastructure.
- 8.17 Whilst the site is located outside of the Town Centre, it does lie close by the boundary and moreover in close proximity of many recent residential developments not only in Petersfield Avenue but at the former Akzo Nobel site on the east side of Wexham Road, where flatted schemes have been approved. Therefore, the principle of flatted development at this specific location is considered to be acceptable and not inappropriate in the context of the site.
- 8.18 Both the National Planning Policy Framework 2023 and the Local Development Plan seek a wide choice of high-quality homes which

should be considered in the context of the presumption in favour of sustainable development. The site is considered to be located in a sustainable location, as it benefits from access to public transport, including a reasonable walk time to Slough railway station, education, retail, leisure, employment and community facilities.

- 8.19 Paragraph 8 of the NPPF sets out that achieving sustainable development means that the planning system has three over-arching objectives, which are interdependent and need to be pursued in mutually supportive ways. These are an economic objective, a social objective and an environmental objective.
- 8.20 Paragraph 9 of the NPPF stresses that sustainable solutions should take local circumstances into account, to reflect the character, needs and opportunities of each area.
- 8.21 In Core Policy 1 the Council seeks a scale and density of development that will be related to a site's current or proposed accessibility, character and surroundings.
- 8.22 In Core Policy 8 the Council seeks all development to be sustainable, of high-quality design that respects its location and surroundings, in that it should respect the amenities of adjoining occupiers and reflect the street scene and local distinctiveness of the area.
- 8.23 Accordingly, in Core Policy 9 the Council states development will not be permitted where it does not respect the character and distinctiveness of existing townscapes. The impact of the current proposals is considered in section 9.0 below.
- 8.24 Having regard to the National Planning Policy Framework 2023 and the Local Development Plan, there are no objections to the principle of a flatted residential scheme.

## 9.0 **Impact on the character and appearance of the area**

- 9.1 The National Planning Policy Framework 2023 (paragraphs 126 and 130) encourages new buildings to be of a high-quality design that should be compatible with their site and surroundings. This is reflected in Core Policies 8 and 9 of the Core Strategy, and Local Plan Policy EN1.
- 9.2 The site lies in a part of Petersfield Avenue that is showing a considerable degree of change as former small-scale commercial buildings are being superseded by new residential buildings and approved schemes with a consistent height and character.
- 9.3 The proposed design of the scheme would provide a single building on the plot that fronts Petersfield Avenue. The overall scale of the building will rise to 5 storeys, which is reflective of the maximum heights of the

other developments permitted on Petersfield Avenue previously mentioned. The plans show that the proposed top (fifth) floor would be set back from the front wall of the proposal to give it a more subservient character.

- 9.4 It should be noted that the officers' advice in the pre-app in 2022 stated:
- "It is considered that 5 storeys is the absolute maximum of acceptable scale for this site."*
- 9.5 As such, it is considered that the submission is appropriate, as it would not be out-of-keeping with the emerging general massing and scale of the area and therefore are no objections to the scale of development as submitted.
- 9.6 The front elevation comprises a broad central element with small, slightly recessed wings to each side, which avoids the appearance of a slab fronted block, and provides for shadow lines to enliven the appearance of the scheme are characterised by a "base" – the ground floor – a "middle" – the first, second and third floor levels – and a "top" – the floors above.
- 9.7 The character of the scheme derives from its articulation of a "base" – the ground floor – a "middle" – the first, second, third, fourth floor levels – and a "top" – the fifth floor.
- 9.8 The base provides a well-defined entrance to the flats with generous proportions featuring a floor-to ceiling glazed style. To the far western side is the opening to serve the ramp to the lower ground floor level functions. The headroom of the under-croft matches the height of the ground floor window openings.
- 9.9 The horizontality of the middle accentuates an orderly and well-mannered pattern of fenestration.
- 9.10 The top features discretely recessed roof level accommodation, set in on both sides and from the rear façade, too. Additionally, it is proposed to have a different finishing material to further differentiate the top and thus ensure the scheme reads as four-storey building given the roof accommodation is set back and in from the main building.
- 9.11 The rear elevation follows the division of base, middle and top as per the front. Whilst the face of the elevation follows across the entire façade – there are no recessed "wings" – the detailing of the brickwork would provide the resemblance of the same feature as the front.
- 9.12 The side elevations are blank faces with the western elevation having a different brick to create a series of three panels at the base level in response to the under-croft within that part of the building.

- 9.13 The proposed building is sited with a gap between each of the adjacent plots. That on the west side would be wider given the angle of that boundary and the consequent arrangement of the vehicular access.
- 9.14 To the front, the whole building would be set back from the northern footway along Petersfield Avenue to afford the upper ground floor accommodation the scope for defensible space behind a boundary wall.
- 9.15 It is noted that the height, siting and the relationship of this scheme follows and mimics the approved schemes at BMW House, Grace House and Thomas House. As such, it is considered that the overall impression of the scheme would not lead to harm to the general feel and visual amenities of the locality.
- 9.16 Based on the above, it is considered that these current proposals would comply with the requirements of the National Planning Policy Framework 2023, Policy EN1 of the Local Plan for Slough March 2004 (Saved Policies), Core Policy 8 of The Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document.
- 9.17 In NPPF terms, the design of the proposals is afforded moderate positive weight in the planning balance given the development results in an improvement to the current low-grade environmental appearance of the streetscene and due to the efficient and effective use of land.

10.0 **Impact on amenity of neighbouring occupiers**

- 10.1 The National Planning Policy Framework 2023 encourages new developments to be of a high-quality design that should provide a high quality of amenity for all existing and future occupiers of land and buildings. This is reflected in Core Policy 8 of the Core Strategy and Local Plan Policy EN1.
- 10.2 As is set out above at 8.4, the NPPF in paragraph 187 stresses the requirement on an Local Planning Authority to ensure new development should be integrated effectively with local businesses and community facilities and that existing businesses and facilities should not have unreasonable restrictions placed on them as a result of development permitted after they were established.
- 10.3 As stated above, the flank elevations would be blank. Therefore, it is considered that these proposals would not prejudice the future redevelopment of the Slough Tyre Centre or Kia Motors site were proposals be brought forward for those plots in due course. Moreover, it is considered that these residential proposals do not conflict with the advice of paragraph 187 of the NPPF.
- 10.4 There are existing residential properties to the north of the site, with a separation distance of approximately 16 metres to the boundary at the

north, which abuts the rear gardens of these dwellings on St Pauls Avenue. Moreover, it is noted that there would be a separation distance of over 50 metres to the rear windows of those dwellings to the north.

10.5 Whilst it is noted that the proposals entail a taller building that would involve elevated outlook from the rear facing accommodation, it is considered that this distance would provide a suitable degree of separation in principle and that should be no direct overlooking impact to these neighbours' homes and no perception of overlooking.

10.6 A Daylight and Sunlight Study was submitted with the application, which demonstrates that there would be no significant impact on the amenities to neighbouring residential properties. In terms of daylighting, there would be no reduction in the lit area of the rooms assessed at the homes in St Paul's Avenue. In terms of sunlight received, all rooms assessed would meet the annual and winter tests. In terms of the gardens, it is noted that each would still receive two hours of sunlight over at least 50% of the garden area and thus comply with the BRE standards.

10.7 In conclusion, it is considered that there would not be harm for neighbouring properties and therefore the proposal is considered to be consistent with Core Policy 8 of the Local Development Framework Core Strategy and Policy EN1 of the Adopted Local Plan, and the requirements of the National Planning Policy Framework 2023.

10.8 In NPPF terms, the impact on neighbours is a neutral consideration in the planning balance.

11.0 **Mix of housing**

11.1 The National Planning Policy Framework (2023) seeks to deliver a variety of homes to meet the needs of different groups in the community. This is largely reflected in local planning policy in Core Strategy Strategic Objective C and Core Policy 4.

11.2 The proposals would provide a mix of one, two and three bedroom flats, as follows:

1-bed/2persons = 5 flats  
2-bed/3persons = 7 flats  
2-bed/4persons = 4 flats  
3-bed/4persons = 4 flats  
3-bed/5persons = 2 flats

So, given the location of the site and its particular circumstances, it is considered that the proposed mix would be appropriate and that the proposals are therefore acceptable.

11.3 In NPPF terms, the proposed provision of 22 homes is afforded moderate positive weight in the planning balance.

12.0 **Living conditions for future occupiers of the development**

12.1 The National Planning Policy Framework 2023 encourages new developments to be of a high-quality design that should provide a high quality of amenity for all existing and future occupiers of land and buildings. This is reflected in Core Policy 8 of the Core Strategy and Local Plan Policy EN1.

12.2 All of the units would meet the Council's internal space standards, as set out in the Technical Housing Standards – Nationally Described Space Standards 2015 (as amended).

12.3 In terms of the levels of daylight, aspect, and outlook, it is considered that each unit would have satisfactory levels of amenity for future occupiers.

12.4 The proposed building would have communal entrance on the Petersfield Avenue frontage providing a lobby to a wheelchair accessible lift and stair core.

12.5 In terms of amenity provision: The scheme includes some outdoor seating for future occupants use at the western side of the building and at lower ground floor level to the rear but no other on-site landscaped area. Additionally, each of the twenty-two flats would each benefit from a private balcony. Those four units at ground floor level would have a balcony extended across the face of the internal accommodation. The three dual aspect units at 1<sup>st</sup>/2<sup>nd</sup>/3<sup>rd</sup> floor levels would have front facing and a rear facing balcony; whilst the single facing units at those levels would have a single balcony with the aspect of the respective unit itself. The one single-aspect unit at roof (4<sup>th</sup>) level would have a north facing terrace; whilst the other two roof level units would benefit from "wrap-around" terraces.

12.6 In terms of noise intrusion: The application was submitted with a Noise Report undertaken by an appropriate expert. This has been reviewed by the Council's Environmental Quality Officer, who comments are set out in full at 6.5 above. No objection on noise grounds are raised, subject to conditions.

12.7 The applicant's Noise Report report sets out that the assessment was based on an external noise level survey and additional measurements of articulated and 12-tonne lorries. This data was used to calculate the required sound insulation performance of the façade elements in order to achieve good amenity standards. It demonstrated that the average noise level limits during the day and the night-time will be readily achieved as will the night-time maximum noise level limit.



- 12.8 The Noise Report concluded that the recommended internal noise level criteria can be achieved. This is despite a slight intensification of the use of a storage facility opposite and despite the moderate levels of noise from traffic and other activity in Petersfield Avenue. The development proposals can therefore enable a good standard of amenity to be achieved for future residents.
- 12.9 The Council's EQ Officer has confirmed these comments and suggested that subject to conditions, as set out below at 26.0, the scheme is acceptable.
- 12.10 Although there would be some issue of noise at the frontage of the site due to heavy goods traffic on Petersfield Avenue, it is considered that the availability of a balcony would be an advantage for future occupiers, as they would have the choice of its use when they feel the need and circumstances are acceptable to them.
- 12.11 So, on balance, it is concluded that the living conditions for future occupiers in this case would have been considered satisfactory and thus to have been in accordance with the requirements of the NPPF 2023, Core Policy 8 of Council's Core Strategy, and Policy EN1 of the Adopted Local Plan.
- 12.12 In NPPF terms, the quality of residential accommodation is a neutral consideration in the planning balance.
- 13.0 **Crime Prevention**
- 13.1 The National Planning Policy Framework 2023 and Policy EN5 of the adopted Local Plan states all development schemes should be designed; so, as to reduce the potential for criminal activity and anti-social behaviour.
- 13.2 The access for would have a good level of natural surveillance. A condition requiring details of the measures to be incorporated to reduce and prevent criminal activity would be adequate to deal with the communal nature of the lobby.
- 14.0 **Highways and Parking**
- 14.1 The National Planning Policy Framework states that planning should seek to promote development that is located where the need to travel will be minimised and the use of sustainable transport modes can be maximised. Development should be located and designed where practical to create safe and secure layouts which minimise conflicts

between traffic and pedestrians and where appropriate local parking standards should be applied to secure appropriate levels of parking.

- 14.2 This is reflected in Core Policy 7 and Local Plan Policy T2 (Parking Restraint) which set out:
- in terms of commercial car parking that development proposals should not increase parking unless required for local safety or operational reasons; and,
  - in terms of residential car parking that the level will be appropriate to both its location and scale whilst taking account of local parking conditions, impact on street scene, need to overcome local road safety problems and protect amenities of adjoining residents.
- 14.3 Paragraph 111 of the National Planning Policy Framework 2023 states that: *'Development should only be prevented or refused on transport grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe'*.
- 14.4 The proposed access would be taken on the western side of the frontage and a redundant crossover would then be removed. The details of boundary treatment will ensure appropriate levels of visibility can be created on the basis of low level means of enclosure. The HA has confirmed the layout and access are acceptable subject to conditions set out below at 26.0.
- 14.5 It is noted that the scale of the scheme would lead to a low level of traffic generation. A comparison with the level of traffic generated by the former use of the site does not raise concerns.
- 14.6 It is noted that the application site is located within a short walking distance of Slough railway and bus stations with a high level of public transport accessibility. Furthermore, its proximity to the Town Centre will enable future residents to access key amenities on foot.
- 14.7 As such, it considered appropriate to apply a lower figure than the Council's standard for car parking provision in this case, as has been done in the vicinity of the application site on Petersfield Avenue and on those sites that lie close to the northern side of the designated Town Centre boundary along Stoke Road and Mill Street to the west, where ratios from 0.5 per unit have been applied.
- 14.8 The application proposes 22no. parking spaces at lower ground floor level to the rear. This produces a provision of one space per flat, which was the ratio approved at BMW House and Thomas House, both close-by to the west. So, although the proposal falls short of the standard set out in the development plan, the Developer's Guide SPD is clear that these standards should be applied flexibly for residential development in very accessible locations, such as this, in compliance with Core Policy 7

as set out above at paragraph 14.2. The HA sets out in 6.1 above that the quantum of parking in this case is acceptable.

- 14.9 The proposals include the provision of a loading bay within the lower ground floor level at the rear to enable the reception of deliveries, goods and services. The HA is satisfied with this arrangement as noted in 6.1 above.
- 14.10 In line with the greater need for pedestrian accessibility, due to the reduced level of on-site parking due to its location, the HA require a contribution towards upgrading the pedestrian routes to these accessible facilities, including a new zebra crossing on Petersfield Avenue in the vicinity of the site.
- 14.11 The proposals show the provision of secure but communal cycle storage facilities in accordance with the Council's standards. The HA is satisfied with this provision subject to details pursuant to a condition, as set out below at 26.0.
- 14.12 The proposals include an enclosed bin and recycling facility, which is close to the highway, in accordance with the Council's standards. The HA is satisfied with the scheme as noted above in 6.1.
- 14.13 Based on the above, and subject to the conditions set out below, it is considered that the proposals would not lead to severe harm to highways users and thus are considered to be in accordance with the requirements of the NPPF and comply with Policies T2 and T8 of the adopted Local Plan and Core Policy 7 of the Core Strategy.

#### 15.0 **Flooding & Drainage**

- 15.1 According to the Environment Agency's flood maps, the site is located in Flood Zone 1. It is at low risk of tidal, fluvial, groundwater flooding, surface water flooding and flooding from artificial sources. As the site is located in Flood Zone 1, the proposals do not require a Flood Risk Assessment.
- 15.2 Since April 2015, major developments have been required to provide measures that will form a Sustainable Drainage System. It has been recognised that Sustainable Drainage Systems (SuDS) are an effective way to reduce the impact of urbanisation on watercourse flows, ensure the protection and enhancement of water quality and encourage the recharge of groundwater in a natural way.
- 15.3 The National Planning Policy Framework (2023) states that the surface run-off from site cannot lead to an increase from that existing. Slough's Strategic Flood Risk Assessment states that surface water should be attenuated to Greenfield run-off rates. In the scenario where infiltration

techniques are not possible, attenuation will be required in order to reduce surface water run-off.

- 15.4 Paragraph 169 of the NPPF 2023 requires major developments to incorporate SuDS unless there is clear evidence that this would be inappropriate.
- 15.5 The Government has set out minimum standards for the operation of SuDS and expects there to be controls in place for ongoing maintenance over the lifetime of the development.
- 15.6 Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document states that development must manage surface water arising from the site in a sustainable manner which will also reduce the risk of flooding and improve water quality.
- 15.7 Submission documentation setting out the applicant's drainage strategy was forwarded to the Council's consultant, who acts as the Local Lead Flood Authority (LLFA). Conditions sought by the LLFA to ensure the scheme meets with appropriate standards are duly set out in the Recommendation at 26.0.
- 15.8 In NPPF terms, the meeting of the requirement for adequate and satisfactory response to the potential impacts of the scheme is a neutral consideration in the planning balance.

16.0 **Trees & Landscaping**

- 16.1 Paragraph 174 of the NPPF requires new development to minimise impacts on biodiversity and provide net gains in biodiversity. Core Policy 9 relates to the natural environment and requires new development to preserve and enhance natural habitats and the biodiversity of the Borough, including corridors between biodiversity rich features.
- 16.2 Firstly, it is noted that the application site does represent a habitats site, as set out in paragraphs 179-181 of the NPPF.
- 16.3 Secondly, that given its existing uses and structures, the application site does not represent a site of biodiversity or part of an ecological network.
- 16.4 The scheme entails a new residential block set in larger plot with some opportunity for soft landscaping on the margins at the side boundaries.
- 16.5 As such, a condition on the selection of an appropriate tree and shrub species, as well as, the management and future maintenance regime are set out in the Recommendation at 26.0.

16.6 In NPPF terms, these proposals afford little weight in the planning balance.

17.0 **Habitats**

17.1 In accordance with the Natural Environment and Rural Communities Act 2006 Local Planning Authorities have a statutory duty to show regard for conserving biodiversity in the exercise of all public functions.

17.2 Paragraph 180 of the NPPF 2021 states that when determining planning applications, if significant harm to biodiversity cannot be avoided or adequately mitigated or as a last resort compensated for then planning permission should be refused. It also states that opportunities to incorporate biodiversity improvements in and around the developments should be encouraged, especially where this can secure measurable net gains for biodiversity. Core Policy 9 of the Core Strategy relates to the natural environment and requires new development to preserve and enhance natural habitats and the biodiversity of the Borough.

17.3 Regulation 61 of The Conservation of Habitats and Species (Amendment) Regulations 2017 (as amended), requires the local planning authority to make an appropriate assessment of the implications of a particular proposal, alone or in combination with other plans or projects on any likely significant effect on a European Site designated under the Habitats Directive.

17.4 Evidence put forward within the Footprint Ecology report 'Impacts of urban development at Burnham Beeches SAC and options for mitigation: update of evidence and potential housing growth, 2019' recognises that new housing within 5.6km of the Burnham Beeches Special Area of Conservation (SAC) can be expected to result in an increase in recreation pressure.

17.5 The site is located approximately some 5 km (as the crow flies) from the Burnham Beeches Special Area of Conservation (SAC) and therefore falls within the potential 5.6km development impact zone as proposed within the evidence base carried out by Footprint Ecology.

17.7 The applicant has submitted a Habitat Regulations Assessment as part of the application which has concluded that an appropriate assessment is required as the proposal is likely to have a significant impact on Burnham Beeches SAC. The Council has a mitigation solution, in place with a fee of £570 per dwelling towards enhancements and proposals at Upton Court Park for development such as this where an appropriate assessment identifies a requirement for such mitigation. The matter can be linked to the completion of a section 106 agreement and therefore, the recommendation includes a requirement for the mitigation package to be secured by the Council.

## 18.0 **Energy & Sustainability**

- 18.1 Core Policy 8 combined with the Developers Guide Part 2 and 4 requires both renewable energy generation on site and BREEAM/Code for Sustainable Homes. The Developers Guide is due to be updated to take account of recent changes and changing practice. In the interim, to take account of the withdrawal of Code for Sustainable Homes new residential buildings should be designed and constructed to be better than Building Regulations (Part L1a 2013) in terms of carbon emissions. Specifically designed to achieve 15% lower than the Target Emission Rate (TER) of Building Regulations in terms of carbon emissions.
- 18.2 The application included an Energy Strategy Report which concludes that the proposed scheme can achieve a 75.19% carbon reduction over Building Regulation Part L 2021 TER.
- 18.3 The residential units proposed will have heating and hot water provided through a combination of air-source heat pumps and photovoltaics. This proposal is acceptable.
- 18.4 The energy proposals are considered to be acceptable in planning terms subject to a condition that would require development to be implemented in accordance with the proposals in the applicant's submitted statement.
- 18.5 In NPPF terms, the meeting of the requirement for adequate and satisfactory response to the potential impacts of the scheme is a neutral consideration in the planning balance.

## 19.0 **Air Quality**

- 19.1 Core Policy 8 of the Core Strategy seeks development to be located away from areas affected by air pollution unless the development incorporates appropriate mitigation measures to limit the adverse effects on occupiers and other appropriate receptors. The proposal should not result in unacceptable levels of air pollution. This is reflected in the National Planning Policy Framework which also goes on to require any new development in Air Quality Management Areas and Clean Air Zones is consistent with the local air quality action plan.
- 19.2 The Council has adopted Low Emission Strategy on a corporate basis, which is a local air quality action plan incorporating initiatives to be delivered by the Council and will set the context for revising the Local Development Plan Policies. Measures in the Low Emission Strategy include reducing traffic, requiring electric charging points, and low emission boilers within new developments. The Low Emission Strategy is a material planning consideration, but it does not form part of the current local development plan.

- 19.3 The application site is not situated within an Air Quality Management Area (AQMA), therefore there will not be an unacceptable exposure to air pollution for future occupiers of the development, as such an Air Quality Assessment has not been submitted as part of the application.
- 19.4 There are identified areas where there could be a temporary impact during the construction phase, such as dust, however these can be mitigated through the approval of a construction management plan. Further mitigation to reduce air quality is through the provision of EV charging points which will be secured by condition.
- 19.5 On the basis of the above considerations there are no objections in respect of air quality impacts.
- 19.6 In NPPF terms, the meeting of the requirement for adequate and satisfactory response to the potential impacts of the scheme is a neutral consideration in the planning balance.

20.0 **Heritage Issues**

- 20.1 As reported above, there are no heritage assets nearby and the site does not lie in a conservation area.

21.0 **Land Contamination**

- 21.1 Paragraph 183 of the NPPF sets out that the LPA should ensure policies and decisions ensure a site is suitable for its proposed use taking account of grounds conditions and any risks arising from land instability and contamination. Core Policy 8 states that development shall not be located on polluted land.
- 21.2 The submission is accompanied by a Phase 1 Desk Study that concludes that:

*“Overall, the level of risk ranged between Unlikely and High. Consequently, a High level of risk has been assigned to the site in respect of the proposed development. This is primarily due to the site history and the proposed residential end use of the site.*

*It is likely however, through remediation and/or the installation of protective measures within the development, that the site can be safely developed and there is no reason to refuse planning on the findings of this report.*

*The assessment assumes the worst possible case regarding infilled land and nature of surrounding industrial land uses; such conditions may in practice be better.”*

21.3 Therefore, the Council's Land Contamination Officer considers that it would be appropriate and satisfactory to impose conditions relating to carrying out the further investigative works to ensure safety during redevelopment and for future occupiers

21.4 In NPPF terms, the meeting of the requirement for adequate and satisfactory response to the potential impacts of the scheme is a neutral consideration in the planning balance.

## 22.0 **Infrastructure and Section 106 requirements**

22.1 Core Policy 10 of the Core Strategy states that development will only be allowed where there is sufficient existing, planned or committed infrastructure. All new infrastructure must be sustainable. Where existing infrastructure is insufficient to serve the needs of new development, the developer will be required to supply all reasonable and necessary on-site and off-site infrastructure improvements.

22.2 The proposals entail the introduction of 22 new residential units. As such, the scheme would trigger affordable housing and educational contributions under the Council's policies, as set out in the Developer's Guide. The Guide indicates that a scheme of 22 units would entail a financial contribution off-site and the figure is stated in para 22.4.

22.3 As set out above, the Highway Authority have identified the need for improvements in the local pedestrian network, which would entail the introduction of a Zebra Crossing in the vicinity of the current proposals. A contribution has been calculated that is based upon the amount per trip previously agreed for a zebra crossing in relation to a scheme for the land at the rear of Theale (SBC ref. P/12934/018). Accordingly, the sum required from this current scheme of some £37,097.

22.4 The site falls within 5.6 km of Burnham Beeches. Therefore, a contribution is needed for mitigation of the increased population of the Borough upon the Burnham Beeches SAC, which would fall under the framework agreed between the Council and Natural England. This equates to a sum of £570 per new residential unit for schemes of over 10 new units. In this case a sum of £7,410 is required.

22.4 The following Section 106 contributions are therefore required:

<b>Off-site provision – financial contributions</b>	
Education	£86,591
Highway infrastructure	£37,097
Burnham Beeches SAC mitigation	£7,410
Affordable housing off-site contribution	£345,784
<b>Total</b>	<b>£476,882</b>



22.5 The applicant's agent has confirmed their client's agreement to these sums.

23.0 **Presumption in favour of sustainable development/Tilted Balance**

23.1 The application has been evaluated against the Local Development Plan and the National Planning Policy Framework 2023 (NPPF) and the Local Planning Authority (LPA) has assessed the application against the core planning principles of the NPPF and whether the proposals deliver "sustainable development."

23.2 The LPA cannot demonstrate a Five-Year Land Supply and therefore the presumption in favour of sustainable development tilted in favour of the supply of housing, as set out in Paragraph 11 of the NPPF and refined in case law, should be applied.

23.3 In the application of the appropriate balance, it is considered that there are some benefits from the scheme.

- The provision of 22 residential units in a sustainable location should be given substantial positive weight.
- Initially, the scheme shall create employment at the construction stage, which is given some slight positive weight.
- Subsequently, the occupiers of the proposed accommodation would help to support local facilities and services; so there would be economic benefits arising from the implementation of these proposals, which is given some slight positive weight.

The application includes the provision of some financial contributions towards education, highways/transport infrastructure, which is considered to be a positive benefit. This should be given positive weight, as the benefit would include the local populous in addition to the residents within the development.

23.4 In considering the impacts, it has been noted that the current scheme would result in the loss of employment generating floorspace in an Existing Business Area to which moderate harm would be attributed too. However, this impact has to be placed in terms of the thrust of the NPPF and the emerging land use arising from approved schemes in this locality, as set out in paragraphs 8.10 - 8.13 above.

23.5 As is the case with proposals when para 11 of the NPPF is engaged, the application does present a balanced case.

23.6 Therefore, in coming to a conclusion, officers have given due consideration to the benefits of the proposal in providing a net gain of 22no. dwellings towards the defined housing need at a time where there

is not a Five-Year Land Supply within the Borough, as well as, some economic benefits and the positive contribution to the streetscene and character of the area as a result of the improvement to the quality of the environment. These factors create a range of limited, moderate and considerable benefits which weigh in favour of the development in the planning balance.

- 23.7 On the basis of the arguments above, it is considered that the benefits of the current scheme would significantly and demonstrably outweigh the identified impacts when assessed against the policies in the Local Development Plan and the NPPF taken as a whole.

## **24.0 Equalities Considerations**

- 24.1 Throughout this report, due consideration has been given to the potential impacts of development, upon individuals either residing in the development, or visiting the development, or whom are providing services in support of the development. Under the Council's statutory duty of care, the local authority has given due regard for the needs of all individuals including those with protected characteristics as defined in the 2010 Equality Act (e.g.: age (including children and young people), disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation. In particular, regard has been had with regards to the need to meet these three tests:

- Remove or minimise disadvantages suffered by people due to their protected characteristics;
- Take steps to meet the needs of people with certain protected characteristics; and;
- Encourage people with protected characteristics to participate in public life (et al).

- 24.2 The proposal would be required to meet with Part M of the Building Regulations in relation to space standards and occupation by those needing wheelchair access. Furthermore, were the scheme to have been acceptable in all other terms, a condition would have been set out to ensure level thresholds at any entrance to the block.

- 24.3 It is considered that there will be temporary (but limited) adverse impacts upon all individuals, with protected characteristics, whilst the development is under construction, by virtue of the construction works taking place. People with the following characteristics have the potential to be disadvantaged as a result of the construction works associated with the development e.g.: people with disabilities, maternity and pregnancy and younger children, older children and elderly residents/visitors. It is also considered that noise and dust from construction has the potential to cause nuisances to people sensitive to

noise or dust. However, measures under other legislation covering environmental health should be exercised as and when required.

24.4 In conclusion, it is considered that the needs of individuals with protected characteristics have been fully considered by the Local Planning Authority exercising its public duty of care, in accordance with the 2010 Equality Act.

25.0 **PART C: RECOMMENDATION**

25.1 Having considered the relevant policies of the Development Plan set out below, and representations that have been received from consultees and the community, and all other relevant material considerations, it is recommended the application be delegated to the Planning Manager:

A) Approval subject to:

- (i) The satisfactory completion of a Section 106 Agreement to secure affordable housing, education and Burnham Beeches contributions, funding towards a zebra crossing (via a Section 278 agreement) that is required to mitigate the impact of the development.
- (ii) Finalising conditions and any other minor changes;

or

B) Refuse the application if the completion of the Section 106 Agreement is not finalised by 31 May 2024 unless a longer period is agreed by the Planning Manager, or Chair of the Planning Committee.

26.0 **PART D: LIST of CONDITIONS and INFORMATIVES**

1. Time Limit

The development hereby permitted shall be commenced within three years from the date of this permission.

REASON: To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light

of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

## 2. Approved Plans

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved unless otherwise agreed in writing by the Local Planning Authority.

- (a) Unnumbered/undated Location Plan; Recd On 31/05/2023
- (b) Drawing No. 01G; Dated 31.05.2023; Recd On 31/05/2023
- (c) Drawing No. 03G (Existing Floor Plan); Dated 31.05.2023; Recd On 31/05/2023
- (d) Drawing No. 04G (Existing Elevations); Dated 31.05.2023; Recd On 31/05/2023
- (e) Drawing No. 04G (Proposed Lower Ground Floor Plan); Dated 31.05.2023; Recd On 31/05/2023
- (f) Drawing No. 05G; Dated 31.05.2023; Recd On 31/05/2023
- (g) Drawing No. 06G; Dated 31.05.2023; Recd On 31/05/2023
- (h) Drawing No. 07G; Dated 31.05.2023; Recd On 31/05/2023
- (i) Drawing No. 08G; Dated 31.05.2023; Recd On 31/05/2023
- (j) Drawing No. 09G; Dated 31.05.2023; Recd On 31/05/2023
- (k) Drawing No. 10G; Dated 31.05.2023; Recd On 31/05/2023
- (l) Drawing No. 11G; Dated 31.05.2023; Recd On 31/05/2023
- (m) Drawing No. 12G; Dated 31.05.2023; Recd On 31/05/2023
- (n) Drawing No. 47384BWLS-01; Dated 01/02/23; Recd On 31/05/2023
- (o) Daylight & Sunlight Report by Anstey Horne ref no. WW/SA/ROL01075; Dated 11<sup>th</sup> May 2023; Recd On 31/05/2023
- (p) Flood Risk Assessment by Simpson tws ref. no. P22-1189 issue.1; Dated 28.01.2023; Recd On 31/05/2023
- (q) Habitat Regulations Assessment by Windrush Ecology ref. no. W5155; Dated 02/05/2023; Recd On 31/05/2023
- (r) Noise Intrusion Assessment by Scotch Partners LLP ref. no. A01324 Rp1; Dated 22/5/23; Recd On 31/05/2023
- (s) Phase 1 Contamination Assessment “Desk Study” by Land Science ref: no. LS6724 Version A; Dated 11<sup>th</sup> April 2023; Recd On 31/05/2023
- (t) Planning Statement by Walsingham Planning; Dated May 2023; Recd On 31/05/2023
- (u) Sales & Marketing Report by On The Market; Dated May 2023; Recd On 31/05/2023
- (v) Transport Statement by RPG ref. no. 2023/7046/TS01 issue 1; Dated 12/05/2023; Recd On 31/05/2023

REASON: To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area in accordance with the requirements of the National Planning Policy Framework 2023 and to comply with the Policies in the Development Plan.

## 3. New finishes to building works

Prior to the commencement of development, samples of new external finishes and materials (including, reference to manufacturer, specification details, positioning, and colour, as well as, bonding were appropriate) to be used in the construction of the external envelope of the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority and the development shall be carried out in accordance with the details approved.

REASON: To ensure a satisfactory appearance of the development so as not to prejudice the visual amenities of the locality in accordance with the requirements of the National Planning Policy Framework 2023 and Policy EN1 of The Adopted Local Plan for Slough 2004.

#### 4. New surface treatments

Prior to the commencement of the development hereby approved, the external materials to be used in the laying out of communal areas within the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority before the scheme is commenced on site and the development shall be carried out in accordance with the details approved.

REASON: To ensure a satisfactory appearance of the development so as not to prejudice the visual amenities of the locality in accordance with the requirements of the National Planning Policy Framework 2023 and Policy EN1 of The Adopted Local Plan for Slough 2004.

#### 5. Drainage (SuDS #1)

Before any above ground works commence a detailed design of surface water drainage scheme for the site based on sustainable drainage principles and an assessment of the hydrological and hydro geological context of the development should be submitted to and approved in writing by the local planning authority. The scheme shall subsequently be implemented in accordance with the approved details before the development is completed.

The scheme shall include:

- i) Details (i.e., designs, diameters, invert and cover levels, gradients, dimensions and so on) of all elements of the proposed drainage system, to include pipes, inspection chambers, outfalls/inlets, and attenuation structures
- ii) Details of the drainage system are to be accompanied by full and appropriately cross-referenced supporting calculations which will include a 10% allowance for urban creep.
- iii) Cross sections of the control chambers (including site specific levels mAOD) and manufacturers' hydraulic curves should be submitted for all hydrobrakes and other flow control devices.

- iv) Detailed scheme for the ownership and scheduled maintenance for every element of the surface water drainage system.
- v) Confirmation of site-specific soil conditions to confirm or exclude use of infiltration solutions.

REASON: To reduce the risk of flooding both on and off site in accordance with the requirements of the National Planning Policy Framework 2023 and Policy 5 of the Core Strategy for Slough Borough Council by ensuring the satisfactory means of surface water attenuation and discharge from the site and to ensure the future maintenance of drainage systems associated with the development.

#### 6. Drainage (SuDS #2)

No development shall take place until a detailed scheme for the ownership and maintenance for every element of the surface water drainage system proposed on the site has been submitted to and approved in writing by the Local Planning Authority and the maintenance plan shall be carried out in full thereafter.

Details are required of which organisation or body will be the main maintaining body where the area is multifunctional (e.g., open space play areas containing SuDS) with evidence that the organisation/body has agreed to such adoption.

The scheme shall include:

- a maintenance schedule setting out which assets need to be maintained, at what intervals and what method is to be used.
- a site plan including access points, maintenance access easements and outfalls.
- maintenance operational areas to be identified and shown on the plans, to ensure there is room to gain access to the asset, maintain it with appropriate plant and then handle any arisings generated from the site.
- details of expected design life of all assets with a schedule of when replacement assets may be required..

REASON: To ensure the future maintenance of drainage systems associated with the development in accordance with the requirements of the National Planning Policy Framework 2023 and Policy 5 of the Core Strategy for Slough Borough Council by ensuring the satisfactory means of surface water attenuation and discharge from the site.

#### 7 - Construction Management Plan (CMP)

Prior to the commencement of the development hereby approved, a Construction Management Plan shall be submitted to and approved in

writing by the Local Planning Authority. The Construction Management Plan shall include the following details:

1. A site set up plan displaying hoarding/fencing extents, vehicle and pedestrian access points during construction, provision for storage of materials, waste and recycling facilities/areas, contractor parking, turning space for construction vehicles, unloading area for deliveries, site office and wheel cleaning facilities during the construction period.
2. Construction vehicles and to comply with Euro VI Emissions Standard as a minimum and machinery to comply with Table 10 of the Low Emissions Strategy Guidance.
3. Delivery hours and working hours. Deliveries shall be made outside peak hours of 08:00 – 09:00 and 17:00 – 18:00, and outside of 14:30 – 15:30 where the development is located in proximity to a school.
4. Details of traffic management measures to control deliveries to site and pedestrian movements on footways in proximity to the site in order to minimise the impact of construction on the safe operation of the surrounding highway network.
5. Vehicle routing plan for HGVs. HGVs shall avoid weight restrictions and AQMAs and local schools at collection/drop off time.
6. Details of dust control measures and wheel washing facilities to be provided on site.
7. Confirmation of whether any abnormal loads will be required for the construction or demolition. If so, the LHA must be notified of any abnormal loads at the following location:  
<https://www.slough.gov.uk/licences-permits/abnormal-loads/1>.

The plan shall thereafter be implemented as approved before development begins and be maintained throughout the duration of the construction works period.

REASON: In the interest of minimising danger and inconvenience to vehicular traffic and pedestrian highway users in accordance with the requirements of the National Planning Policy Framework 2023 and Policies 7 and 8 of the Core Strategy 2008.

## 8 Sound attenuation and ventilation

Prior to commencement of development, full details of the glazing specification, in accordance with the recommendations outlined in the Noise Intrusion Assessment by Scotch Partners LLP ref. no. A01324 Rp1, must be submitted to and approved in writing by the Local Planning Authority.

Prior to commencement of development, a ventilation strategy, including detail of proposed plant and location of inlet and outlets, must be submitted to and approved in writing by the Local Planning Authority.

Should a mechanical ventilation system not be installed, a full overheating assessment must also be provided.

The development shall be carried out in full accordance with the approved details prior to first occupation and shall be retained as such at all times in the future.

REASON: In the interest of mitigating noise from road traffic to provide acceptable living conditions of future residents, to comply with policy 8 of the Core Strategy 2006 - 2026 adopted 2008, and the requirements of the National Planning Policy Framework 2023.

## 9 Phase 2 Intrusive Investigation Method Statement

The findings of the Phase 1 Desk Study having identified the potential for contamination, development works shall not commence until an Intrusive Investigation Method Statement (IIMS) has been submitted to and approved in writing by the Local Planning Authority. The IIMS shall be prepared in accordance with current guidance, standards and approved Codes of Practice including, but not limited to, BS5930, BS10175, CIRIA 665 and BS8576. The IIMS shall include, as a minimum, a position statement on the available and previously completed site investigation information, a rationale for the further site investigation required, including details of locations of such investigations, details of the methodologies, sampling and monitoring proposed.

REASON: To ensure that the type, nature and extent of contamination present, and the risks to receptors are adequately characterised, and to inform any remediation strategy proposal in accordance with the requirements of the National Planning Policy Framework 2023 and Policy 8 of the Core Strategy 2008.

## 10 Phase 3 Quantitative Risk Assessment and Site-Specific Remediation Strategy

Development works shall not commence until a Quantitative Risk Assessment (QRA) has been prepared for the site, based on the findings of the intrusive investigation. The risk assessment shall be prepared in accordance with the Land Contamination: Risk Management (LCRM) and Contaminated Land Exposure Assessment (CLEA) framework, and other relevant current guidance. This must first be submitted to and approved in writing by the Local Planning Authority and shall as a minimum, contain, but not limited to, details of any additional site investigation undertaken with a full review and update of the preliminary Conceptual Site Model (CSM) (prepared as part of the Phase 1 Desk Study), details of the assessment criteria selected for the risk assessment, their derivation and justification for use in the assessment, the findings of the assessment and recommendations for further works.



Should the risk assessment identify the need for remediation, then details of the proposed remediation strategy shall be submitted in writing to and approved by the Local Planning Authority. The Site Specific Remediation Strategy (SSRS) shall include, as a minimum, but not limited to, details of the precise location of the remediation works and/or monitoring proposed, including earth movements, licensing and regulatory liaison, health, safety and environmental controls, and any validation requirements.

REASON: To ensure that potential risks from land contamination are adequately assessed and remediation works are adequately carried out, to safeguard the environment and to ensure that the development is suitable for the proposed use in accordance with the requirements of the National Planning Policy Framework 2023 and Policy 8 of the Core Strategy 2008.

#### 11 Remediation Validation

No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy condition shall be occupied until a full Validation Report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority. The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Site-Specific Remediation Strategy condition above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation from a Building Control Regulator that all such measures have been implemented.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health and in accordance with the requirements of the National Planning Policy Framework 2023 and Policy 8 of the Core Strategy 2008.

#### 12 EV Charging facilities

Prior to the commencement of any development hereby approved, details of 22 active electric vehicle charging points (Type 2 socket and be rated to at least 3.6kW 16amp 0 7kW 30amp single phase) shall be submitted to and approved in writing by the local planning authority. The approved details shall be fully installed and the active charging points shall be fully operational prior to the first occupation of the development and be retained in good working order at all times in the future.

REASON: To ensure vehicle parking is provided and encourage up-take of electric vehicle use, in accordance with the requirements of the National Planning Policy Framework 2023, Policy T2 of the Adopted Local Plan (2004), Policies 7, 8 and 10 of the Core Strategy 2008, and

the guidance contained in the Council's Developer's Guide Part 3 (2008).

### 13 Landscaping

Construction of the building above ground floor level shall not commence on site until details of a Landscaping Strategy and Management Plan in conjunction with a detailed bee-friendly landscaping and tree planting scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the type, density, position and planting heights, along with staking/guying, mulching, feeding, watering and soil quality, of new trees and shrubs, and details of hardsurfaces which shall include compliance with the surface water drainage mitigation as approved under conditions 5 and 6 of this planning permission.

On substantial completion of the development, the approved scheme of hard landscaping shall have been constructed. The approved scheme of soft landscaping shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON: In the interests of the visual amenity of the area in accordance with the requirements of the National Planning Policy Framework 2023 and Policy EN3 of The Adopted Local Plan for Slough 2004.

### 14 Boundary Treatment

Construction of the building above ground floor level shall not commence on site until details of the proposed boundary treatment including position, external appearance, height and materials of all boundary walls, fences and gates have been submitted to and approved by the Local Planning Authority. The development shall not be occupied until the approved boundary treatment has been implemented on site and retained thereafter.

REASON: In the interests of the visual amenity of the area and to reduce opportunities for crime and anti-social behaviour in accordance with the requirements of the National Planning Policy Framework 2023, Policies EN1 and EN3 of The Adopted Local Plan for Slough 2004, Core Policies 1 and 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008).

### 15 Crime Prevention

No development above ground floor slab shall commence until a secure access strategy and secure letter/parcel drop strategy in line with the principles of Secured by Design and in consultation with Thames Valley Police has been submitted and approved in writing by the local planning authority. The development shall be carried out in accordance with the approved details and shall not be occupied or used until written confirmation of Secured by Design accreditation has been submitted to the Local Planning Authority. The approved security measures shall be retained thereafter.

REASON: In order to minimise opportunities for crime and anti-social behavior in accordance with Policy EN5 of The Adopted Local Plan for Slough 2004 (saved polices) and Core Policies 8 and 12 of the adopted Core Strategy 2006-2026, and the requirements of the National Planning Policy Framework 2023.

#### 16 Drainage (SuDS #3)

No Occupation shall take place until the Verification Report for the installed surface water drainage system for the site, based on the approved Flood Risk Assessment by Simpson tws ref. no. P22-1189 issue.1, has been submitted in writing by a suitably qualified drainage engineer and approved by the Local Planning Authority The report shall include:

- a) Any departure from the agreed design is keeping with the approved principles
- b) Any As-Built Drawings and accompanying photos
- c) Results of any Performance testing undertaken as a part of the application process (if required / necessary)
- d) Copies of any Statutory Approvals, such as Land Drainage Consent for Discharges etc.
- e) CCTV Confirmation that the surface water drainage system is free from defects, damage, and foreign objects
- f) Confirmation of adoption or maintenance agreement for all SuDS elements as detailed within the drainage strategy is in place.

REASON: To ensure the installed Surface Water Drainage System is satisfactory and in accordance with the approved reports for the development site.

#### 17 Bins & Recycling facilities

The refuse and recycling facilities as shown on the approved plans shall be provided on site prior to occupation of the development and retained thereafter.

REASON: To ensure that there is adequate refuse and recycling storage to serve the development in accordance with the requirements of the National Planning Policy Framework 2023 and Policy EN1 of The Local Plan for Slough 2004.

## 18 Access

No part of the development shall be occupied until the new means of access has been sited and laid out in accordance with the approval plans and constructed in accordance with Slough Borough Council's Adopted Vehicle Crossover Policy.

REASON: To ensure that adequate access provision is available to serve the development to prevent highway congestion and safety issues and to protect the amenities of the area in accordance with the requirements of the National Planning Policy Framework 2023, Core Policy 7 and 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008 and Policy T2 of The Adopted Local Plan for Slough 2004.

## 19 Visibility

No part of the development shall be occupied until the visibility splays shown on the approved drawings have been provided on both sides of the access and the area contained within the splays shall be kept free of any obstruction exceeding 600 mm in height above the nearside channel level of the carriageway.

REASON: To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access in accordance with the requirements of the National Planning Policy Framework 2023.

## 20 Layout

The scheme for parking, manoeuvring and the loading and unloading of vehicles shown on the submitted plans shall be laid out prior to the initial occupation of the development hereby permitted and that area shall not thereafter be used for any other purpose.

REASON: To enable vehicles to draw off, park, load/unload and turn clear of the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway in accordance with the requirements of the National Planning Policy Framework 2023.

## 21 Car Parking Provision

Prior to the development hereby approved first being brought into use, 22 no. car parking spaces shall be provided and made available for use in connection with the residential development and maintained for the parking of cars on a communal basis thereafter. The car parking spaces shall not be used for any separate business, commercial or residential use.

REASON: To ensure that adequate on-site parking provision is available to serve the development to prevent highway congestion and safety issues by overspill of parking onto the surrounding streets in accordance with the requirements of the National Planning Policy Framework 2023, Core Policy 7 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008 and Policy T2 of The Adopted Local Plan for Slough 2004.

## 22 Cycles storage

Prior to the first occupation of the development hereby permitted details of a secure cycle store providing 22 cycle parking spaces in accordance with the Local Planning Authority's "Cycle Parking Standards" shall be submitted to for approval by the Local Planning Authority. The cycle parking shall be provided in accordance with these details and shall be retained for this purpose thereafter.

REASON: To ensure that there is adequate cycle parking available at the site in accordance with the requirements of the National Planning Policy Framework 2023, Policy T8 of The Local Plan for Slough 2004, and to meet the objectives of the Slough Integrated Transport Strategy.

## 23 External Site Lighting

No part of the development hereby permitted shall be occupied until a scheme has been submitted to and approved in writing by the Local Planning Authority for external site lighting including details of the lighting units, levels of illumination and hours of use. No lighting shall be provided at the site other than in accordance with the approved scheme and retained thereafter.

REASON: In the interests of safeguarding the amenities of neighbouring properties and to ensure safer access and use of the shared cycle/pedestrian/motor vehicular areas throughout the site in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008, Policy EN5 of The Adopted Local Plan for Slough 2004 (saved polices), and the requirements of the National Planning Policy Framework 2023.

## 24 Privacy screening

No part of the development hereby permitted shall be occupied until a scheme of privacy screening to the sides of the roof level terraces to prevent conflicts of privacy within the proposed development has been submitted to and approved by the Local Planning Authority. The approved scheme shall be installed on site in accordance with the

approved details prior to the first occupation of the development and retained thereafter.

REASON In the interests of the visual and neighbour amenity, and to ensure no overlooking into the neighbouring sites to help ensure that there would not prejudice wider redevelopment in accordance with the requirements of the National Planning Policy Framework 2023; Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008, and Policies EN1 and H9 of The Adopted Local Plan for Slough 2004 (saved polices).

#### 25 Gates

Prior to first occupation of the development, the gates shall be setback a minimum of 5.5 metres from the back edge of the footway, maintained and retained thereafter.

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development in accordance with the requirements of the National Planning Policy Framework 2023.

#### 26 Level Access

The ground floor entrance doors to any part of the development shall not be less than one metre wide and the threshold shall be at the same level to the paths fronting the entrances to ensure level access. Level thresholds shall be provided throughout the development between the flatted residential units and the external amenity/balconies and the main lobbies.

REASON: In order to ensure the development provides ease of access for all users, in accordance with the requirements of the National Planning Policy Framework 2023, Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008).

#### 27 No new windows

Notwithstanding the provisions of the Town & Country Planning (General Permitted Development) (England) Order 2015, (or any Order or Statutory Instrument revoking and re-enacting that Order), no windows, other than those hereby approved, shall be formed in any elevations of the development.

REASON: To ensure the visual character and appearance of the facades are preserved and to ensure the development does not prejudice the future development of adjoining lands; so, as to protect the privacy of neighbouring properties and to protect the visual amenities of

the area in accordance with the requirements of the National Planning Policy Framework 2023, Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008, and Policy EN1 of The Adopted Local Plan for Slough 2004 (saved polices).

### 28 Telecommunications Equipment

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking or re-enacting that Order with or without modification), no aerials, antennae, satellite dishes or related telecommunications equipment shall be erected on any part of the development hereby permitted.

REASON: To ensure that the visual impact of telecommunication equipment can be considered in accordance with the requirements of the National Planning Policy Framework 2023, Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008).

### 29 Thames Water – Piling Method Statement

No piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

REASON: The proposed works will be in close proximity to underground sewerage utility infrastructure.

### 30 Energy

The development hereby approved shall be implemented to fully include the proposals and measures set out in the Energy Strategy Report by Syntegra ref. no. 23-10406; Dated 19/05/2023; Recd On 31/05/2023 and retained thereafter.

REASON To ensure that the development provides sustainable energy benefits to the development in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 - 2026, Development Plan Document, December 2008.

## INFORMATIVES

1. It is the view of the Local Planning Authority that the proposed development does improve the economic, social and environmental conditions of the area for the reasons given in this notice; so it is in accordance with the National Planning Policy Framework.

2. Highways

The applicant will need to apply to the Council's Local Land Charges on 01753 875039 or email to [0350SN&N@slough.gov.uk](mailto:0350SN&N@slough.gov.uk) for street naming and/or numbering of the unit/s.

Prior to commencing works the applicant will need to enter into a Section 278 Agreement of the Highways Act 1980 with Slough Borough Council for the implementation of the works in the highway works schedule. The applicant should be made aware that commuted sums will be payable under this agreement for any requirements that burden the highway authority with additional future maintenance costs.

The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding, skip or any other device or apparatus for which a licence must be sought from the Highway Authority.

The applicant is advised that if it is intended to use soakaways as the method of dealing with the disposal of surface water then the permission of the Environment Agency will be necessary.

The development must be so designed and constructed to ensure that surface water from the development does not drain onto the highway or into the highway drainage system.

3. Ventilation Strategy

Should mechanical ventilation be utilised, the system is to be designed by a suitably qualified engineer or M&E consultant. The system shall be designed so the sound levels from any external plant or inlet/outlets do not exceed the background noise level at any noise receptor. This may require further assessment once the type and location of system has been specified.

4. Thames Water

### Waste Comments

We would expect the developer to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer. Groundwater discharges typically result from construction



site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Should the Local Planning Authority be minded to approve the planning application, Thames Water would like the following informative attached to the planning permission: "A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing [trade.effluent@thameswater.co.uk](mailto:trade.effluent@thameswater.co.uk) . Application forms should be completed on line via [www.thameswater.co.uk](http://www.thameswater.co.uk). Please refer to the Wholesale; Business customers; Groundwater discharges section.

With regard to SURFACE WATER drainage, Thames Water would advise that if the developer follows the sequential approach to the disposal of surface water we would have no objection. Management of surface water from new developments should follow Policy SI 13 Sustainable drainage of the London Plan 2021. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. Should you require further information please refer to our website. <https://developers.thameswater.co.uk/Developing-a-large-site/Apply-and-pay-for-services/Wastewater-services>.

There are public sewers crossing or close to your development. If you're planning significant work near our sewers, it's important that you minimize the risk of damage. We'll need to check that your development doesn't limit repair or maintenance activities, or inhibit the services we provide in any other way. The applicant is advised to read our guide working near or diverting our pipes. <https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>.

The proposed development is located within 15 metres of our underground waste water assets and as such we would like the following informative attached to any approval granted. The proposed development is located within 15 metres of Thames Waters underground assets and as such, the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures. <https://developers.thameswater.co.uk/Developing-a->

large-site/Planning-your-development/Working-near-or-diverting-our-pipes. Should you require further information please contact Thames Water. Email: [developer.services@thameswater.co.uk](mailto:developer.services@thameswater.co.uk) Phone: 0800 009 3921 (Monday to Friday, 8am to 5pm) Write to: Thames Water Developer Services, Clearwater Court, Vastern Road, Reading, Berkshire RG1 8DB

Thames Water would advise that with regard to WASTE WATER NETWORK and SEWAGE TREATMENT WORKS infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

#### Water Comments

If you are planning on using mains water for construction purposes, it's important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at [thameswater.co.uk/buildingwater](http://thameswater.co.uk/buildingwater).

The applicant is advised that their development boundary falls within a Source Protection Zone for groundwater abstraction. These zones may be at particular risk from polluting activities on or below the land surface. To prevent pollution, the Environment Agency and Thames Water (or other local water undertaker) will use a tiered, risk-based approach to regulate activities that may impact groundwater resources. The applicant is encouraged to read the Environment Agency's approach to groundwater protection (available at <https://www.gov.uk/government/publications/groundwater-protection-position-statements>) and may wish to discuss the implication for their development with a suitably qualified environmental consultant.

Thames Water recommends the following informative be attached to this planning permission. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

**Slough Borough Council**

<b>REPORT TO:</b>	Planning Committee
<b>DATE:</b>	29 <sup>th</sup> November 2023
<b>SUBJECT:</b>	Slough Local Plan Annual Monitoring Report 2022/2023
<b>CHIEF OFFICER:</b>	Dan Ray
<b>CONTACT OFFICER:</b>	Pippa Hopkins, Group Manager
<b>WARD(s):</b>	All
<b>APPENDICES:</b>	A - Draft Annual Monitoring Report 2022/23

**1 Summary and recommendations**

- 1.1 The purpose of the report is to inform Members about the results of the annual planning policy monitoring for 2022/23. This includes information on housing, employment, retail and environment.

**Recommendation(s)**

- 1.2 The Committee is requested to resolve that:
- a) The results of the Annual Monitoring Report 2022/23 be noted.
  - b) The Annual Monitoring Report 2022/23 be published on the Council's website.

**2 Report****Introduction**

- 2.1 Local Planning Authorities have a statutory obligation to produce and publish an annual planning monitoring report. While some content is statutory, other elements are a matter for individual councils to decide. It is important for monitoring the effectiveness of current local plan policies and informing the preparation and production of the new Local Plan.
- 2.2 Slough's Annual Monitoring report (AMR) provides information and statistics on housing, employment, retail and the natural and built environment. This AMR covers the period from 1 April 2022 to 31 March 2023 with some updates where this is appropriate and are available. It includes progress of the implementation of the Local Development Scheme and planning policies included in the development plan documents.
- 2.3 The AMR will be published on the Council's website but as it is factual report it is not subject to consultation.

## Housing

- 2.4 In 2022/23 there were 322 net housing completions in Slough. This figure is much less than the annual average over the last 4 years even though it includes continuing completions on the large Horlicks Quarter site. All of the 322 homes except 28 were built on sites of 10 or more homes. The 28 figure is lower than average over the past few years but there are over 104 homes under construction on small sites.
- 2.5 Of the total 325 gross completions 4 homes were created by conversion of existing homes and 97 (30%) were from a change of use. 3 homes were lost through conversion or demolition.
- 2.6 At the end of March 2023 there were 1,121 dwellings under construction including over 300 on the first phase of the Horlicks site. This is a bit less than each of the last 2 years but higher than average over the last 6 years. Excluding the Akzo Nobel site (which is unlikely to progress) there were 1,659 dwellings with planning consent but not yet started (including outline permissions). Over 700 of that figure is phase 2 of the Horlicks Quarter development which is expected to start in 2024.
- 2.7 The National Planning Policy Framework requires Local Planning Authorities to identify a supply of specific deliverable sites sufficient to provide five years' worth of housing against their housing requirement or Local Housing Need (LHN) figure with an additional buffer to ensure choice and competition in the market for land.
- 2.8 The buffer for Slough is currently 20% instead of the normal 5% as the Housing Delivery Test result for Slough is below a critical threshold. At the 1<sup>st</sup> of April 2023 the 5 year housing land supply figure was 2.6 years. As a 5 year supply cannot be demonstrated the Council has to apply the 'presumption in favour of sustainable development' when determining planning applications. As of January 2023, 38 per cent of all English planning authorities were unable to demonstrate a five-year housing land supply, with the situation worse in the south east than other regions ([www.planningresource.co.uk](http://www.planningresource.co.uk) Feb 2023).
- 2.9 100% of the gross housing completions in 2022/23 were on previously developed sites (brownfield). This figure tends to fluctuate over the years depending upon which sites are coming forward, but last year's figure is broadly consistent with the last 5 years.
- 2.10 As required by Planning Practice Guidance (PPG), since 1 April 2016 the Council keeps a custom and self-build homes register, details of which are publicised on the website. The register holds information on individuals and associations of individuals

who are seeking to acquire serviced plots of land. The number of individuals on the self- build register at July 2023 is 248.

- 2.11 183 new build dwellings were in the affordable category in 2022/23. This is the largest number since 2015 and represents 57% of total net completions. Most were on the Horlicks Quarter development.
- 2.12 The results of monitoring for 2022/23 showed that 6% of housing completions were houses and 94% flats or maisonettes. This trend is likely to continue in the future. This reflects the effectiveness of the policy in the Core Strategy that seeks to firstly direct new development to the town centre and other urban areas, where flats are generally acceptable, and secondly ensuring that development in the suburban areas consists predominantly of family housing.

### Employment

- 2.13 The monitoring shows that there was a net gain of 3,628 square metres of employment floor space in 2022/23. Most of the new floorspace (55,719 sqm) was data centre (36,054 sqm) or warehouse/general industry use (18,258 sqm). Space lost was mostly older industrial/commercial space along with some office space lost to residential use (6,572 sqm). These figures do not include closure of part of Queensmere retail shopping centre. Although there was a modest net gain of employment floorspace for 22/23 previous years showed a general trend of loss of employment space.
- 2.14 The Thames Valley and South-East Office Market Report (2023) produced by Lambert Smith Hampton shows that Slough has 6.5 years supply of offices and 8385 sqm of available space. The Slough office Market was one of the hardest hit in the South East due to the pandemic. Low levels of office take up since 2018 have yet to recover.
- 2.15 Slough Trading Estate continues to be doing well under SEGROs management. The estate continues to be a priority investment area. While it is prosperous as an employment land area, the number of office and manufacturing jobs is likely to continue to decrease. For example, more datacentres are being built but they have a different employment profile, and do not employ as many people as the businesses lost.
- 2.16 The Trading Estate still benefits from being a Simplified Planning Zone (SPZ). This helps existing and potential new occupiers plan for the future with more certainty by allowing certain types of development to take place without the need to apply for separate planning permission provided they meet specified conditions set out in the SPZ scheme. The Slough Trading Estate SPZ is due to expire in November 2024; and Cabinet approved negotiation of a replacement earlier in 2023. During 2022/23, six SPZ schemes were permitted, and two data centre developments were completed.

## Retail

2.17 Slough has not had any major retail scheme completions in the last few years. The Annual retail survey of the town centre and district centres was undertaken in February 2023.

### Slough Town Centre Retail

2.18 The results showed Slough Town Centre had a retail unit vacancy rate of 22%. The figure for Queensmere for 22/23, below, is higher than 21/22 as half of the centre was closed in January 2022. The retail vacancy rate does not reflect the quality of the retail offer. Many of the units in the shopping centres have no signage or permanent shop fit out and appear to be pop ups with temporary/short term lets. The nature of the offer appears to serve a local population with a lot of takeaways and small independent grocery shops.

- The Queensmere has 51% of units vacant.
- The Observatory has 26% of units vacant.
- The High Street has 9% of units vacant.

### District Centres Retail

2.19 Retail vacancy surveys were conducted in Slough district centres at Langley, Farnham Road and Chalvey.

2.20 Chalvey had a retail vacancy rate of 11%, Langley and Farnham Road both 2%. These are low retail vacancy rates which show these centres are healthy and vibrant.

## Appeal Decisions

2.21 Appeal decisions are regularly reported to Planning Committee. The AMR looks at whether there are any patterns in appeal decisions that can support future decision making. There were 41 appeals in Slough in the 12 months from April 2022 with 9 allowed by Inspectors.

2.22 There were 7 appeals on enforcement cases, with 6 upheld.

2.23 25 of the appeals were for householder or advertisement applications. Of the 9 allowed the key issues related to design, character of the area or amenity which tend to be site specific judgements.

2.24 There were 16 Appeals on strategic matters such as housing and employment. The upholding of decisions for Major Applications demonstrate the current Local Plan is working well and continues to be compliant with the NPPF. This is useful as it shows our specialist policies, such as those to protect family housing from conversion to flats, and refusing poor design continue to be justified.

## Progress on Planning Policy and Slough Local Plan

2.25 The Council has completed its Regulation 18 phase of the Local Plan preparation and the next step will be gathering evidence in support of the Regulation 19 phase. The Levelling up and Regeneration Act received Royal Assent on 26<sup>th</sup> October 2023 and will come in to force on the 26<sup>th</sup> December. This sets out changes that affect the Local Plan process but they are dependent on secondary legislation that has no implementation date at present. Adopting a Local Plan is a lengthy process and requires significant financial and resource investment. At present the team are progressing with updating the Simplified Planning Zone (approved by Cabinet in February 2023) and in March '23 was awarded a Grant to implement the new Duty for 'Biodiversity Net Gain'. It has also begun liaison on how to resource health facilities via S106.

### Duty to Cooperate

2.26 The Local Planning Authority (LPA) has a duty to engage actively and, on an ongoing basis, with specified partners on their Local Plans including adjoining Councils. Slough does this via responding to consultations, engaging with officers and Members as needed. These discussions are focused on significant strategic cross boundary matters such as employment, housing and flooding to support the evidence base in policy development.

2.27 In 22/23 regular meetings were held with Royal Borough of Windsor and Maidenhead and Buckinghamshire Council to discuss cross boundary issues in particular the pressure for land to meet housing need in the sub-region. Members of Slough Borough Council and Buckinghamshire Council met in March '23 to demonstrate agreement to work towards a Statement of Common ground on each other's Plans.

2.28 RBWM and SBC worked together to finalise and publish The Wider Area Growth Study. That is an evidence base document that sets out where areas of land may be suitable for housing growth. Importantly these have not considered the local Planning policy constraints and opportunities that would need to be applied before the sites could be promoted or delivered, as this will be for each LPA to do.

### Summary

2.29 The Council has a duty to publish monitoring information on the Local Plan. This report highlights key points and statistics from the draft AMR from April 2022 to March 2023. The AMR will be published on the Council's web site once approved by the Committee.

## **3 Implications of the Recommendation**

### Financial implications

There are no financial implications for this report.

Legal implications

There are no legal implications for this report.

Risk management implications

Minimal – The Council has a duty to publish monitoring information for the Local Plan but failure to do so does not incur any sanctions.

Environmental implications

There are no environmental implications for this report.

Equality implications

There are no equality implications for this report.



# Slough Local Plan Annual Monitoring Report 2022/23

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November 2023

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**Slough**  
Borough Council

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# 1 Introduction

## 1.1 Overview

1.1.1 This Annual Monitoring Report covers the period from 1st April 2022 to 31st March 2023, and meets the statutory duty for continuous monitoring of important information.

1.1.2 The Annual Monitoring Report reports on key information relating to planning policy issues in Slough, in terms of the effect of adopted policies. It provides monitoring information on housing, employment, retail and town centres and built and natural environment. The report also relates to our emerging Local Plan.

1.1.3 The document is set out in topic-based sections including housing, retail, employment, environment. Each section has some key facts, discusses key issues and then the relevant indicators that have been monitored.

1.1.4 For further information on planning policy for Slough, please visit the [Council's website](#).

## 1.2 Adopted and emerging Planning Policy

1.2.1 Slough's Local Development Plan consists of the Core Strategy (adopted in December 2008); Site Allocations (adopted in 2010), Saved Policies from the Local Plan 2004, and the Proposals Map (2010). There are also saved policies from the Minerals and Waste Plans. These will remain the statutory Development Plan until the new Local Plan reaches adoption.

1.2.2 Please refer to Section 10 for the Slough new Local Plan.

## 1.3 Setting the Scene

1.3.1 Slough is highly urbanised and has a small geographical area of 32.5 km<sup>2</sup>.

1.3.2 Slough's population at the time of the 2021 Census was 158,500 – an increase of 13.0%, from around 140,200 in 2011. This growth is markedly higher than the national average of 6.6%. This confirms that there are a greater number of residents in the borough than has previously been included in recent [ONS estimates](#) (149,577 as of mid-2020).

1.3.3 Slough is one of the smallest Councils by area and one of the most densely built-up areas in the country. There is shortage of land for new development (Centre for Cities, 2020).

1.3.4 There are now 52,423 households in Slough containing at least one person – a growth of only 3.3% on 2011, compared to 6.1% in England and Wales. Slough also saw England's largest percentage-point rise in the proportion of households including a couple with dependent children (from 23.3% in 2011 to 27.5% in 2021).

1.3.5 Slough is the third most densely populated local authority in the Southeast, with 4,871 usual residents per square kilometre (48.7 per hectare compared to 45.8 in 2011). In the latest census, around 87,400 Slough residents said they were born in England. This represented 55.1% of the local population. The figure has risen from around 83,200 in 2011, which at the time represented 59.4% of Slough's population. (India was the next most represented, with (10.8%), and Pakistan (9.1%).

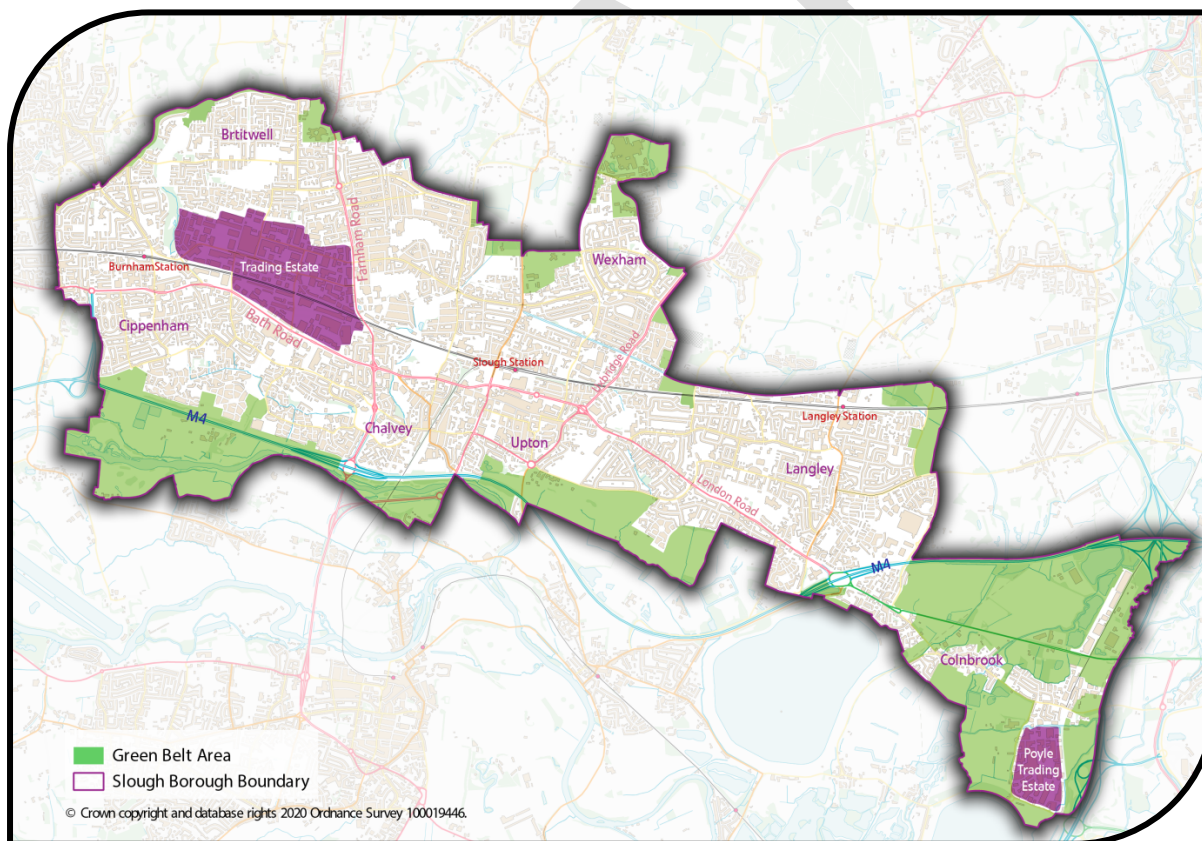
1.3.6 In addition to the town centre, there are two district centres at Farnham Road and Langley. These are supplemented by a network of neighbourhood centres. There are three retail parks which are concentrated in the west and centre of the Borough.

1.3.7 There are three railway stations on the Great Western Railway line: Burnham, Slough (town centre), and Langley (Figure 1 below), all of which are on the Elizabeth line.

1.3.8 The M4 motorway (Jn 5-7) runs along the southern boundary of Slough acting as an east west bypass, with the M25 (Jn 15) easily accessible from the eastern boundary. The M4 creates a barrier to the land to the south. The A4 is the main spine road through the town but is so congested at times that it does not cater for through traffic. The main north south route through Slough is the A355 which goes towards Windsor in the south and Beaconsfield/M40 in the north. The A412 leads towards Uxbridge.

1.3.9 One of Slough's biggest advantages is its proximity to Heathrow airport. This not only provides a lot of employment on site but is a big boost to the local economy. The airport also creates some environmental problems and can add to congestion, particularly in the eastern part of the Borough.

**Figure1: A map of Slough**



## 2 Housing

### 2.1 Key facts

- Slough has one of the youngest populations in the country (average age 34) with a high proportion of children (29% children aged 0-17) and families who will need new homes in the future (only 10% 65 or over).
- Slough has some of the highest levels of overcrowding with each person having on average 27.2 m<sup>2</sup> of space in a home compared to an average of 36.5m<sup>2</sup> in other towns and cities.
- Slough saw the South East's largest percentage-point rise in the proportion of privately rented homes (from 24.3% in 2011 to 30.7% in 2021).

## **2.2 Housing issues**

2.2.1 There is a shortage of land for housing. Slough's trajectory continues to show that Slough, over the next 18 years, will have at least a 5,000-housing shortfall compared to current housing need figures. As part of the initial stage of Local Plan preparation further work will be undertaken to confirm the shortfall figure.

2.2.2 Sites with potential for redevelopment for residential use do not always come forward as expected and approved developments do not always get built out at the rate expected. Despite the strong demand for new homes the supply does not meet the Borough needs in terms of numbers and type of home. Economic viability issues usually arise in connection with redevelopment sites i.e. those on previously developed land (brownfield sites). Measured over several years and on average the number of homes granted consent has increased at a greater rate than completion of new homes. The Council is primarily dependent upon firstly the private sector to apply for planning permission and secondly wider economic conditions that encourage landowners to sell land and developers to build out schemes with consent.

2.2.3 There is a significant need for affordable housing and for a range of house types including in particular family housing and homes with gardens. The vast majority of housing completions in the recent past have been in the form of flatted developments.

## **2.3 Meeting housing need**

2.3.1 Housing targets for the Local Plan and the 5-year housing land supply are calculated using the 2021 Standard Methodology published in the National Planning Policy Framework and Planning Guidance. This takes account of household projections and affordability ratios published by the Government to establish a Local Housing Need figure. Every year, new affordability ratios are produced and as a result the local housing need figure changes.

2.3.2 The Local Housing Need figure for Slough applied to the 22/23 housing calculation is 847 homes per annum (average). For this year, 23/24, the latest housing need figure of 825, released in April 23, will be used. When applied to the new Local Plan period, which extends to 2040/41, the total housing need figure as at April 2023 is 16,100 homes.

## **2.4 Housing delivery test**

2.4.1 In 2018, the "Housing Delivery Test" (HDT) was introduced into the planning system as part of the new National Planning Policy Framework. The Housing Delivery Test (HDT) measures net

additional dwellings provided in a local authority area against the homes required. The methodology for calculating the Housing Delivery Test measurement is set out in the Housing Delivery Test Measurement Rule Book, and the Department for Levelling Up Housing and Communities publish the HDT result for each local planning authority in England annually usually in November. However, no announcement has been made since January 2022.

2.4.2 The results of the 2021 Housing Delivery Test which were published in January 2022 show that over the previous three years housing delivered in Slough was 67% of the required level. This means that the 'presumption in favour of sustainable development' applies when deciding planning applications as 67% falls below the current Test threshold of 75%. In 2021 Slough was just above the threshold. Further implications of the test, but no different to last year, are the need to have a 20% rather than a 5% buffer added to the Borough housing need/requirement figure. And for a Housing Delivery Action Plan, prepared in line with national guidance, to be in place (see below section 2.9).

## **2.5 Housing completions**

2.5.1 Table 1 below shows the rate of net housing completions during the Local Plan period since 2006. See table C in the Appendix for the relationship to housing targets.

2.5.2 In 2022/23, there were 322 net housing completions in Slough. This figure is much less than the annual average over the last 4 years even though it includes continuing completions on the large Horlicks Quarter site. All of the 322 homes except 28 were built on sites of 10 or more homes. The 28 figure is lower than average (over the past few years) but there are over 104 homes under construction on small sites.

2.5.3 Of the total 325 gross completions, 4 homes were created by conversion of existing homes and 97 (30%) were from a change of use. 3 homes were lost through conversion or demolition. A list of sites with completions is in Appendix (Table A).

2.5.4 At the end of March 2023, there were 1121 dwellings under construction including over 300 on the first phase of the Horlicks site. This is a bit less than each of the last 2 years but higher than average over the last 6 years. There were 2659 dwellings with planning consent (including outline permissions) but not yet started. Over 700 of that figure is phase 2 of the Horlicks Quarter development which is expected to start in 2024. The number of homes not started is a bit less than each of the last two years. But it should be noted 1,000 of the 'not started' figure is the residential element of the former Akzo Nobel site which is now most unlikely to progress.



**Table 1: Housing net completions since 2006**

<b>Year</b>	<b>Past Completions</b>	<b>Cumulative completions</b>
<b>2006/07</b>	409	409
<b>2007/08</b>	849	1,258
<b>2008/09</b>	595	1,853
<b>2009/10</b>	275	2128
<b>2010/11</b>	249	2,377
<b>2011/12</b>	246	2623
<b>2012/13</b>	182	2805
<b>2013/14</b>	396	3201
<b>2014/15</b>	507	3708
<b>2015/16</b>	789	4497
<b>2016/17</b>	521	5018
<b>2017/18</b>	846	5864
<b>2018/19</b>	534	6398
<b>2019/20</b>	503	6901
<b>2020/21</b>	501	7402
<b>2021/22</b>	532	7934
<b>2022/23</b>	322	8256

## **2.6 Housing trajectory**

2.6.1 The Housing trajectory (Table D in Appendix 1) sets out an estimate of housing supply in the Borough over the new Local Plan period 2020/21-2040/41. It also states the current Local Housing Need figure. That figure is greater than the expected future supply, so the difference means there will be a shortfall of new housing within the Borough in future years as referred to in para 2.2.1 above and 2.6.3 below.

2.6.2 The trajectory estimate is based upon the 5-year housing land supply sites (see below), other developments with consent and sites the Council assess as likely to or have a reasonable prospect of coming forward taking account of the 2010 Site Allocations, emerging Local Plan,

existing planning policies and known landowner/developer interest. The trajectory considers these principal sources of supply:

- Sites with Planning consent for residential units, taken from data in the Council's planning records (hard commitments). This includes full, outline and reserved matters consent plus permitted development and permission in principle.
- Sites allocated for development in the existing Local Plan or Site Allocations Development Plan.
- Sites likely to come forward for development having considered current undecided planning applications, pre apps, Council development/land proposals, the 'call for sites' exercise in 2016 and the Spatial Strategy.
- An estimate for new homes built on small sites (likely to have fewer than 10 homes).

2.6.3 The trajectory figures take account of the likelihood that not all the developments/sites used to compile it will come forward - known as discounting. Before discounting, the trajectory exercise listed sites that could accommodate over 11,000 homes. After discounting this figure, it reduces to about 9,500 homes. Comparing the latter figure to the current Local Housing Need figure of 16,100 indicates a shortfall of well over 5,000 homes. For the latter years of the new Local Plan period, some unidentified sites are expected to come forward but there is no evidence to suggest they would make up the estimated shortfall.

2.6.4 The inputs to the trajectory and housing need figure do vary over time. So the trajectory and associated calculations are reviewed each year at least.

2.6.5 The 'hard commitments' – new homes with planning consent are listed in the Appendix (table B). The list is also the basis of the 5-year land supply figure, see below, but certain sites are excluded to accord with Government guidelines.

## **2.7 Five-year housing land supply**

2.7.1 The National Planning Policy Framework (NPPF para 68) requires Local Planning Authorities to identify a supply of specific deliverable sites sufficient to provide five years' worth of housing against their housing requirement or Local Housing Need (LHN) figure with an additional buffer of 5% to ensure choice and competition in the market for land." However the Housing Delivery Test result for Slough as outlined above requires a 20% buffer to be applied.

2.7.2 Slough's current Local Housing Need figure is equivalent to 825 net additional dwellings per year; this is derived from the standard methodology in the NPPF as stated above.

## 2.8 Five-year Housing land supply calculations

<b>5 year housing land supply</b>	
Annual average Local Housing Need	825
Local Housing Need x 5 years	4125
5 year need figure inclusive of 20% buffer	4950
Annual Local Housing Need inclusive of 20 % buffer (4950/5)	990
5 year identified supply	2607
Number of years supply (2607/990)	2.63
Rounded 5 year land supply	2.6 years

2.8.1 The table above shows that Slough has 53% of the Local Housing Need figure (inclusive of buffer) which equates to a 2.6-year supply as of 1<sup>st</sup> April 2023. Consequently, Slough does not have a five year supply of new housing as defined by the Government. To strictly align with the

2.8.2 definition, this figure currently excludes applications with outline permission or waiting for planning obligations to be signed or sites allocated for development in the Local Plan.

2.8.3 Last year the 5 year supply figure was 2.1 years. The Council intends to review the way it calculates the figure taking account of best practice and what other local authorities do. This might result in a few more sites being included.

2.8.4 Where Local Planning Authorities cannot demonstrate a five year supply of deliverable housing the development plan policies are considered to be out of date. This means that the 'tilted balance' must be applied in determining planning applications for housing development. This requires local planning authorities to apply the 'presumption in favour of sustainable development' which is set out in paragraph 11d of the NPPF. This states that applications should be granted planning permission unless:

- i) the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed, or
- ii) any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

## 2.9 Housing delivery action plan

2.9.1 A Housing Delivery Action Plan was produced in July 2019 and published on the Council's

website. The Plan identifies that many of the reasons for under delivery are outside the control of the Council and it sets out various actions in order to try to improve housing delivery and meet the Housing Delivery Test in future years. These reasons and actions are still substantially relevant. An updated Plan will be produced in the near future when resources are available.

## 2.10 Type of Housing

2.10.1 The results of monitoring for 2022/23 show that 6% of completions were houses and 94% flats or maisonettes.

2.10.2 Table 2 below shows past trends.

**Table 2: Percentage of Houses and Flats completed since 2016.**

Year	% Flats	% Houses
2015/16	66	34
2016/17	67	33
2017/18	75	25
2018/19	83	17
2019/20	90	10
2020/21	90	10
2021/22	94	6
2022/23	94	6

2.10.3 The predominant dwelling type in Slough is traditional two or three bedroom houses. However, as indicated above, most new homes are flats and increasingly so in recent years. Most flatted schemes are a mixture of one and two bedroom properties with a few three bedroom homes.

2.10.4 The predominance of flats reflects, in part, the effectiveness of the policy in the Core Strategy that seeks to firstly direct new development to the town centre and other urban areas, where flats are generally more acceptable, and secondly ensuring that development in the suburban areas consists predominantly of family housing. Since 2014, the relaxed planning regime under Permitted Development rules (also known as Prior Approvals) for change of use from offices to residential has also contributed to the predominance of flats in the centre of Slough.

2.10.5 Last year 97 of total home completions were a change of use from offices. 95 were via the Permitted Development rules. Both figures are less than previous years. Whilst there is a need for all dwelling types, the need for family homes is not being met as well as that for small homes. And the need for traditional family homes with gardens is met even less.

2.10.6 The predominance of flats is likely to continue as very few green field sites are likely to come forward for development in the near future. It is much easier to insist on houses for those sites. And viability issues around redevelopment of existing properties will remain. Plus the relaxed Permitted Development (PD) rules, in a revised form, still exist. Change of use to offices under PD rules are not likely to be as high as the past but other uses can now change to residential use under the recently revised rules. Development via PD rules results in lost employment or retail space and prevents the Council controlling the mix of homes (number of bedrooms per flat) and

seeking affordable housing.

## **2.11 Previously developed land completions**

2.11.1 100% of the gross housing completions in 2022/23 were on previously developed sites (brownfield). This figure tends to fluctuate over the years depending upon which sites are coming forward but last year's figure is broadly consistent with the last 5 years.

2.11.2 Table 3 below shows that the trend is most housing completions are on previously developed land and this has never fallen below 50 percent. This trend is likely to continue as few greenfield sites are being developed now. And as highlighted under type of housing most development takes place in the centre of Slough.

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**Table 3: Previously Developed Land completions**

<b>YEAR</b>	<b>Total Gross Completions</b>	<b>Total Gross PDL Completions</b>	<b>% PDL</b>
2022/2023	325	325	100%
2021/2022	541	508	98%
2020/2021	510	510	100%
2019/2020	514	511	99%
2018/2019	585	585	100%
2017/2018	861	804	93%
2016/2017	598	484	81%
2015/2016	782	536	68%
2014/2015	599	301	50%
2012/2013	190	95	50%
2011/2012	258	175	67%
2010/2011	262	201	77%
2009/2010	285	172	60%
2008/2009	632	198	69%
2007/2008	917	896	98%
2006/2007	475	466	98%
2005/2006	533	498	93%
2004/2005	867	500	58%

## **2.12 Housing mix**

2.12.1 As most new homes were flats, one and two bedroom homes predominate with some studios and some 3 bedroom flats.

2.12.2 It should be noted that 30% of new home completions came via the prior approval/permitted development process for change of use. It is likely that most of these homes would have been 1 or 2 bedroom homes including some bedsits. However applications under the Permitted Development process do not have to include details of mix so precise details cannot easily be gained.

2.12.3 The lack of new family sized housing coming forward in Slough provides further justification

for the Local Plan Spatial Strategy of protecting the existing stock in the suburbs, seeking family homes within the town where feasible and promoting cross border development, such as the northern expansion of Slough in the form of a garden suburb to help rebalance the housing market.

## **2.13 Affordable housing**

2.13.1 Affordable housing is essential in order to meet local housing needs. Affordable housing can be delivered by a combination of house building by the Council, registered providers (mostly housing associations) and private developers. For the latter, affordable housing is normally provided through the planning process by securing a proportion of dwellings on private development sites as affordable housing under Section 106 planning obligations.

2.13.2 Core Policy 4 in the Core Strategy DPD 2006-2026 requires that 30-40% affordable housing on sites of 15 homes or more primarily on site or for smaller sites (14-24 homes) financial contributions for the Council to fund affordable homes elsewhere in the town.

2.13.3 183 new build dwellings were in the affordable category in 2022/23. This is the largest number since 2015 and represents 57% of total net completions. All 183 were secured via the planning system. All are flats and most were on the Horlicks Quarter site.

2.13.4 The supply of affordable homes is affected by a number of factors. Firstly, the availability of Council or Housing Association land and funds to build themselves. Secondly, the scope to insist on affordable housing via planning obligations on private development sites. The latter is affected by whether or not large enough sites come forward in any one year and viability of redevelopment on brownfield sites. Applications now come forward with viability studies which usually show that it is not possible to provide the full policy compliant quota of affordable housing.

2.13.5 The second factor limiting supply is that the Council is unable to get contributions from change of use schemes that have come forward under the Prior Approval/Permitted Development process. Thirdly the ability of Housing Associations to buy homes, offered by private developers under planning obligations, is limited. Government grant funding for Housing Associations purchasing planning obligation housing is not now available. Homes England funding for affordable housing is also limited but is being used on the Horlicks Quarter development.

2.13.6 Regarding affordable housing via planning obligations some contributions are financial rather than homes built on site. Those contributions, for the Council to use on housing, are included in the Planning Obligations section below.

## **2.14 Self-build register**

2.14.1 As required by Planning Practice Guidance (PPG), since 1 April 2016 the Council keeps a custom and self-build homes register, details of which are publicised on the Council's website.

2.14.2 The register holds information on individuals and associations of individuals who are seeking to acquire serviced plots of land. The PPG encourages Local Planning Authorities to publish headline data in their AMR on the demand for self-build or custom housebuilding.

2.14.3 The Council has not set any local eligibility criteria or fees for going on to or staying on the register. As such anyone going on to the register would be on 'Part 1'. There are no financial checks completed at this stage to indicate whether or not people registering can realistically fund

the property they are indicating they would like.

2.14.4 The number of individuals on the self- build register at July 2023 is 241.

2.14.5 For more information on how to join the register please visit [Register for a self-build home – Slough Borough Council](#)

## **2.15 Brownfield land register**

2.14.6 In accordance with the Town and Country Planning (Brownfield Land Register) Regulations 2017 and the Town and Country Planning (Permission in Principle) Order 2017 which came into force in April 2017, the Council is required to prepare and maintain a register of brownfield land that is suitable for residential development.

2.14.7 Regulation 17 of the Town and Country Planning (Brownfield Land Register) Regulations 2017 requires local planning authorities to update the information relating to existing entries in their registers at least once a year and that it may consist of two parts:

Part 1 - all sites which are 'suitable', 'available', and 'achievable' for residential development which could be delivered within 15 years; and

Part 2 - any sites which are given 'permission in principle'. Inclusion on part 2 would grant permission in principle for residential development (the scale to be determined by the Council) and the landowner/developer would have to apply for 'technical details consent' before any development could commence.

2.14.8 Brownfield sites that meet the relevant criteria must be entered in Part 1 of the Register. There are currently no sites classified as Part 2 on the register. The Register can be found on the [Slough website](#)

## **3 S106 planning obligations**

3.1.1 Details on Section 106 planning obligations for 2022/23:

- 11 new planning obligations signed (excludes variations)
- Affordable housing via planning obligations: 183 units completed
- Total financial contributions £675,335 made up of
  - Affordable Housing: £481,053
  - Infrastructure: £194,282

The above relates to all obligations not just those related to housing. The total sum received for both infrastructure and affordable housing is much lower than 21/22 (£ 6,342,642). However that figure was exceptionally high. And it should be noted that the money received in the first half of 2023/24, (approx. £ 2.5 m) is already substantially higher than the total for last year. The annual average figure over the last 10 years is over £ 3m per year.

## **4 Employment**

### **4.1 Key facts**



- Slough Trading Estate Limited has 97.3% Occupancy, and the second-largest hub of data centres in the world (SEGRO 2022 Annual Report).
- Slough is an economic powerhouse.
- Slough hosts a number of corporate headquarters and has one of the highest business start-up rates in the country.
- Slough residents earn £27.8 less per week than the regional average.
- There is a gap of £84 between Slough residents (£657.5) and all those who work in Slough (£741.5) (Nomis Labour Market Profile, Jan 2023 (data for 2022)).
- Slough has a larger proportion of people claiming unemployment benefit (4.9%) than the South East and England averages (Nomis Labour Market Profile, June 2023)
- 6.2% unemployment rate (Nomis, December 2022)

## 4.2 Employment issues

4.2.1 Slough is an economic powerhouse but it doesn't always benefit as much as it should from all of this economic activity. Slough's residents have lower paid salaries when compared to those who are commuting into the Borough who have higher paid salaries.

4.2.2 Without the expansion of Heathrow Airport, which would have provided a significant boost to the local economy, the only major opportunity for employment growth is new offices in the town centre. However, the stock of office buildings in the town has reduced considerably in recent years primarily as a result of change of use to residential use. Also due to the covid pandemic the nature of how some companies are operating is changing. Combined with staff working remotely demand for office space is reduced.

4.2.3 Because of current uncertainty, it is not possible to confidently predict number of jobs required to support the Slough economy. But the plan continues to aim to provide an additional 15,000 jobs in order to meet the needs of the growing resident workforce. Further evidence is required for employment need and this will be produced.

4.2.4 There is a significant demand for land for warehousing and data centres in Slough. This together with the need to plan for additional jobs means land is needed for new industrial/business floorspace – either redevelopment of existing employment/business use land or greenfield land. And as stated above there is also a shortage of land for much needed housing. Much new housing in recent years has been on former employment land. Competition for land is high so land/property values have risen in recent years.

4.2.5 [The Centre for Cities reports](#) on Slough, extract from their research on unemployment levels in the Borough is below.

The chart below compares inflation to changes in wages on a city-by-city basis, and show what this means for the 'real' wage in each place - i.e. whether the average person can buy more or less with their wages as a result of price and wage rises.

### What's the impact of rising prices on money in people's pockets?

In July 2023, national inflation was **6.8%**.

In **Slough**, nominal wages grew by **6.7%** over the same period (year-on-year).

This translates into a **-0.1%** change in real wages in **Slough**, assuming average prices grew at the national inflation rate (year on year).

But the cost of living crisis started at the beginning of 2021.

Comparing current wages to their January 2021 level shows that workers living in **Slough** are still on average **£65.9** a month poorer than they were when the crisis started.

Monthly wages adjusted to inflation and real wage growth (year on year)



#### Sources:

**Claimant count:** ONS, Claimant Count. ONS, Population Estimates. Note: Data differ to NOMIS claimant count rates as latest available population estimates are used to calculate the figures.

**Employees:** ONS, Earnings and employment from Pay As You Earn Real Time Information, seasonally adjusted (PAYE).

**Competition for jobs:** Indeed UK, Job postings and CVs June-August 2020.

**Job postings:** Indeed UK, Job postings February 2020 - July 2021.

**Share of population with high-level qualifications:** ONS, Annual Population Survey, resident analysis. Department for the Economy (DfE NI), District Council Area Statistics for Belfast.

**Wages adjusted to inflation:** ONS, HMRC (PAYE). Loss measured in January 2021 prices.

## 4.3 Completed Employment floorspace

4.3.1 The monitoring shows that there was a net gain of 3,628 square metres of employment floor space in 2022/23. Gross new floorspace was 55,719 sqm.

4.3.2 Of new gross floorspace, the dominant uses are data centres (36,054 sqm) and warehousing/general industrial (18,258 sqm). Two office buildings have been lost to residential use (6,572 sqm). One small office has been lost to education use. The new data centres and warehousing have generally replaced former industrial/business uses or small scale warehouses.

4.3.3 Although there has been a modest net gain in employment floorspace it is likely that that there will have been a net loss of employment in terms of workers. This is because much of the floorspace gained is for data centre use and some office buildings have been lost. Data centre uses generally employ fewer people per unit of floorspace compared to normal office or industrial uses.

4.3.4 In 21/22 there was a net loss of 12,000 of floorspace and the general trend since 2015 has been a gradual loss of employment floorspace.

4.3.5 It should be noted that the above figures do not reflect any new development involving less than 1,000 sqm (minor development). They also do not reflect the closure of part of the Queensmere shopping centre nor the demolition of the former Sainsburys store on Farnham Road.

4.3.6 Regarding the definition of uses, it should be noted that under the Use Classes Order of August 2021, the new use class E (commercial and business services) covers a wide range of uses such as offices, restaurants, retail and leisure plus some industrial and warehouse uses. These used to be categorised separately such as B1a offices, B2 general industry and B8

warehousing. This report identifies the separate uses by name where it is obvious that use is intended on completion. However, the current Use Classes Order provides for more flexibility for changing use without express planning permission including changes within the wide ranging class E. The Government is increasingly using changes to Use Class order and Permitted Development rights to stimulate economic growth.

#### **4.4 Slough Trading Estate and the Simplified Planning Zone (SPZ)**

4.4.1 Slough Trading Estate has benefited from being in single ownership of SEGRO. This has enabled co-ordinated and phased management and redevelopment, and for example the application of a Simplified Planning Zone since 1995.

4.4.2 SEGRO's Slough Trading Estate Limited reports having 581,845sqm of lettable space with 97.3% Occupancy, and the second-largest hub of data centres in the world (SEGRO 2022 Annual Report).

4.4.3 The SPZ is a planning tool which is used across the majority of the Slough Trading Estate to support economic development and encourage investment through simplifying the planning approval process for new industrial buildings. It effectively grants planning permission if all conditions are met.

4.4.4 "Responsible SEGRO" is a part of SEGRO's economic model on the estate. It focuses on three strategic priorities for business, environmental and social value: championing low-carbon growth; investing in local communities and environments; and nurturing talent. Designed to promote 'work packages' to local businesses, their 22 Annual report quoted over £1.5 million was invested into the local economy. Due to the success of this pilot, they will roll out similar programmes during 2023. They have also piloted one of their first 'vertical' industrial units 'V-Park' at Leigh Road.

4.4.5 Karl Storz, the world's leading endoscope manufacturers, has a sales and distribution centre for their UK business on the Estate. In 2021 they added their Training and Technology Centre within their premises, to enhance their training facilities for surgical skills.

4.4.6 The current SPZ was adopted in 2014 and expires in 2024. In March 2022, Cabinet agreed to the Council working with SEGRO to develop a new SPZ and to notify the Secretary of State for Levelling Up, Housing and Communities of the Council's intention to do so. The new SPZ will have an area of 162.3 ha, which is a slight increase from 156ha from the currently adopted SPZ.

4.4.7 Two SPZ schemes were completed and six permitted in 2022/23 are listed below. In addition, it should be noted that a new data centre at 650-660 Ajax Avenue for 11,176 sqm scheme (ref T/153) is nearing completion.

**Table 5 SPZ schemes completed in 2022/23**

Ref	Address	Description
T/149	580 Ipswich Rd	Construction of a three storey 11,176 sq. m data centre for colocation uses with a footprint of 49.8%.
T/152	110 Buckingham Avenue	Proposed development: demolition of 110 Buckingham Avenue (6190 sq. m) and the construction of a new data centre facility of 24,878 sq. m over three floors and plant at ground floor level.

**Table 6 SPZ schemes permitted in 2022/23**

Ref	Address	Description
T/154	160-161 Bestobell Road	Change of use to allow a fibre exchange/data centre (Sui Generis) in addition to the permitted Class B1(C), B2, B8 as well as the installation of two generators, air conditioning units, louvered screening, new fencing and a new gate with a total footprint of 33.8%. No change to the number of existing car parking spaces.
T/155	317-320 Farnham Road	Single building for B8 use with ancillary offices
T/156	2 Buckingham Avenue	Amendments to existing building and plant
T/157	108 Oxford Avenue	Installation of PV Panels to the roof
T/158	111-113 Buckinghamshire venue and 460 Malton Avenue	Proposed development: Single building of 17,100.6 sqm for colocation / Data Centre use with three floors plus 1 floor of plant with 40.8 % footprint.
T159	485 Berkshire Avenue	Proposed development: Single building of 18,792 sqm for colocation / Data Centre use with three floors plus 1 floor of plant with 48% footprint

## 4.5 Office market and development

4.5.1 In the year 22/23, two office buildings were lost to residential use (6,572 sqm) and one small one is now in education use. No new independent office buildings were completed but new warehouse and datacentre buildings include some ancillary office space. There is an outstanding planning permission for the second Future Works office on Wellington Street but there is no indication that it will commence soon. And no other major independent offices are under

construction.

4.5.2 The Thames Valley and South-East Office Market Report (2023) produced by Lambert Smith Hampton shows that Slough has 6.5 year's supply of office floorspace with 8385 sqm of available in 2022. Both figures are higher than the previous year. Headline rents are similar to those of the last few years.

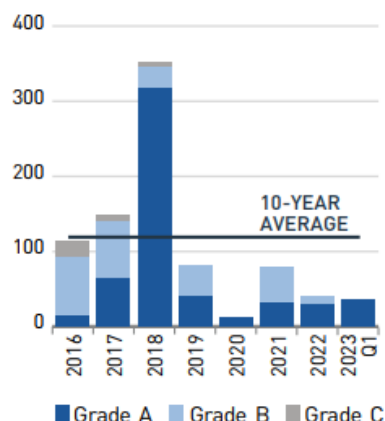
4.5.3 The Slough office market was one of the hardest impacted in the South East due to the pandemic. The snapshot below shows the low levels of office take up since 2018 but there is some activity in the market with some take up of empty office space. However, some of that relates to an office at Ditton park just beyond the Borough boundary.

4.5.4 Most of the office space available in Slough is good quality Grade A or B. There is only a limited supply of grade C office space. This is probably the result of many older offices having been converted to residential use.

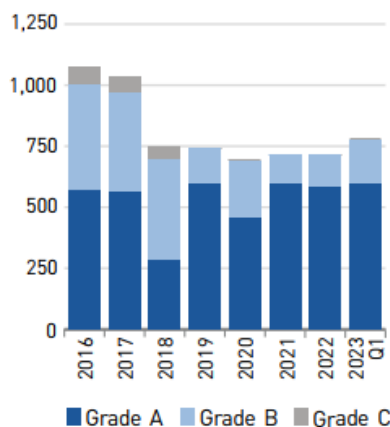
DRAFT

## Snapshot of market activity

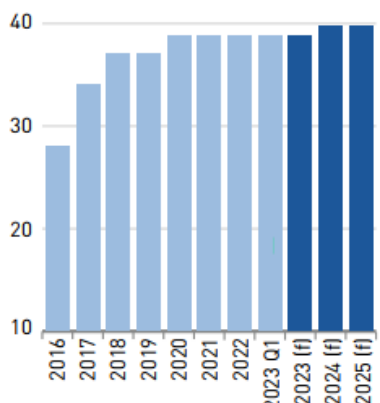
### TAKE-UP (000 SQ FT)



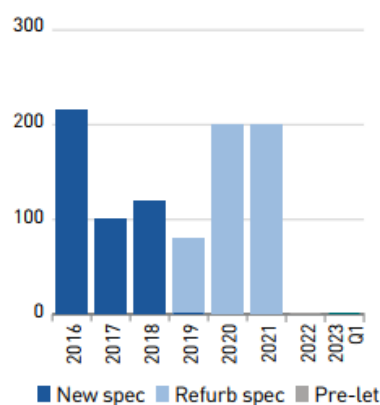
### AVAILABILITY (000 SQ FT)



### PRIME HEADLINE RENT (£ PER SQ FT)



### UNDER CONSTRUCTION (000 SQ FT)



### MARKET AT A GLANCE

12 MONTH TAKE-UP VS  
10-YEAR AVERAGE

-38%

YEARS OF  
SUPPLY

6.5

GRADE A SHARE  
OF SUPPLY

77%

PRIME  
YIELD

6.25%

Q1 2023 HEADLINE RENT  
(PER SQ FT)

£38.75

### KEY SELECTED TRANSACTIONS

Quarter	Property	Size (sq ft)	Occupier	Rent (per sq ft)
2023 Q1	Botanica (2nd Floor)	25,344	Avarto	£29.18
2022 Q2	Charter Court (1st Floor)	12,877	Motorola	£28.00
2023 Q1	Botanica	9,450	Continental Tyres	£32.50
2022 Q4	The Urban Building	8,459	The Old Station Nursery	£27.50
2022 Q2	4 Waterside Court	4,894	Langley Hall Primary Academy Trust	£23.50

Source: LSH Research

## 5 Retail and town centre

### 5.1 Key facts

- Slough Town Centre has a reduced retail and leisure offer due to the partial closure of the Queensmere shopping centre in January 2022
- District Retail centres in Langley and Farnham Road are thriving due to a more specialised offer.

- Since September 2020, Use Class A1, A2 and A3 have been replaced by E(a) (shops), E(b) (restaurants and cafés), E(c) (financial and professional services). A4 and A5 are now sui generis uses. These use classes will be used in future annual monitoring reports.

## 5.2 Slough Town Centre

5.2.1 Slough has a declining retail offer and the future of the shopping centre is uncertain. It is recognised that Slough will no longer be a sub-regional shopping centre and there will be a significant reduction in the amount of retail floorspace in the town centre.

5.2.2 The Queensmere shopping centre was partly boarded up in January 2023 which reduced retail space in the town centre. The outline application for redevelopment of the Queensmere shopping centre was agreed in principle in November 2022, for the demolition of buildings and the phased redevelopment of the site to provide a mixed-use scheme comprising residential floorspace (C3 use and provision for C2 use); flexible town centre uses floor space (Use Class E and Use Class F), provision for office floorspace (Use Class E (g) (i)), supporting Sui Generis town centre uses (including a range of the following uses: pubs, wine bars, hot food takeaway), Sui Generis leisure uses (provision for a cinema or live music venue). This reduces the amount of retail floorspace in the town centre

5.2.3 A retail survey was undertaken in February 2023. The results showed an overall Slough Town Centre vacancy of 22%. This is particularly high due to high levels of vacancies in the shopping centres. The Town Centre retail vacancy survey counts the number of vacant units in the designated primary and secondary retail frontages in Slough Town Centre. The most recent survey in 2023 showed Slough Town Centre had a vacancy rate of 22%. This is mainly due to the large number of retail unit vacancies in the shopping centres. The high street has a low retail vacancy rate and a good range of shops, restaurant and café.

- The Queensmere has 51% of units vacant\*
- The Observatory has 26% of units vacant.
- The Highstreet has 9% of units vacant.

5.2.4 The 51% Queensmere figure is high as half of the shopping centre closed in January 2023.

5.2.5 In addition, the retail vacancy rate does not reflect the quality of the retail offer. Many of the units in the shopping centres and high street have no signage or permanent shop fit out. These are pop ups which are temporary/short term lets.

5.2.6 There has been a loss of national chain stores in Slough town centre ( the High Street Wilkinson store closed in late summer 2023). There has however been an increase in small independent businesses such as takeaways and restaurants, and local grocers selling fresh vegetables and other products to suit the local population's demographic.

## 5.3 District Centres

5.3.1 In contrast to Slough town centre, the Farnham Road and Langley district centres are thriving which offer a more specialised offer. These along with a series of neighbourhood centres and local parades (see Core Strategy Appendix 4) provides accessible local services for the

community.

5.3.2 Retail vacancy surveys were conducted in Slough district centres at Langley, Farnham Road and Chalvey neighbourhood Centre.

5.3.3 Chalvey neighbourhood Centre had a retail vacancy rate of 11%, Langley and Farnham Road District Centres both had a vacancy rate of 2%.

5.3.4 This are low retail vacancy rates which show these centres are healthy and vibrant. The former Sainsbury in Farnham Road has been demolished and replace with a storage facility. The centre, like Chalvey, benefits from a large independent supermarket.

#### 5.4 Retail Parks

5.4.1 Retail parks will be monitored, and retail vacancy survey undertaken in future annual monitoring report as these provide valuable services and retail offer to local communities. These are very vibrant retail parks with a range of town centre uses such as shops, restaurants, coffee shops. A retail vacancy survey will be undertaken in 2023 as a baseline for the 2024 Retail Vacancy survey.

#### 5.5 Slough Town Centre Business Improvement District (Slough BID)

5.5.1 A Business Improvement District (BID) is a geographical area in which the local businesses have voted to invest together to improve their environment.

5.5.2 A Business Improvement District (BID) is a business-led and business-funded limited company, created through a ballot process, to improve a defined area in which a levy is charged on all business rate payers in addition to their normal business rates bill. The BID is funded primarily through this levy but can also draw on other public and private funding streams, using its resources to develop projects that are intended to provide additional services or improvements to the local area.

5.5.3 [Slough BID](#) was established following a successful ballot in 2020 and contains over 400 businesses. The Slough BID will be due for renewal in 2025. The BID is focused on delivering projects within the 5 themes prioritised by the businesses, with the team working closely with the Council:

- Safe and Secure: Helping to deal with crime and anti-social behaviour in the town centre
- Environment: Creating a clean and pleasant environment
- Marketing and Events: Promoting Slough as a welcoming, vibrant town Centre with events for everyone.
- Business Support: Training and networking opportunities, cost reduction through joint procurement and customer loyalty schemes.
- Representation: Being the voice for business, representing business interests and working with other organisations to get the best for the town.

## 6 Appeals submitted to the Planning Inspectorate

### 6.1 Key facts

- 41 Appeals decisions received, 32 dismissed, 9 allowed



- 16 of these concerned strategic matters for the Local Plan regarding housing
- 7 appeals related to enforcement action, with 6 enforcement notices upheld

## 6.2 Number of appeals

6.2.1 Appeal decisions are reported to Planning Committee on a regular basis (see Planning Committee Agenda items on the Council’s website hyperlink [here](#)). The AMR looks at whether there are any lessons to be learnt from appeal decisions. There were 41 appeal decisions received in Slough in the 12 months from April 2022 to March 2023. 32 were dismissed and only 9 allowed by Inspectors.

6.2.2 Enforcement appeals are important as they demonstrate where breaches are being made. At present resources limit the number of cases the enforcement officer can investigate, but the reports of appeals remain useful as an indicator. Appeals are important to demonstrate both the need for the policy to protect family housing, and its premise that housing need is being met and supply is being provided in a way that is not recorded in housing completions.

6.2.3 This table records appeal decisions made between April 2022 and March 2023.

**Table 7 Appeal decisions**

Number of appeals on Local Plan strategic matters – (housing, employment)	Number of appeals on householder/advertisements	Core Policies or Saved Policies referred to.	Total No. of appeals
16	25	EN1, EN2, H14, H15, OSC15 Slough Local Plan, 2004. CP8, CP3, CP4, CP8, CP7 of Core Strategy, 2008	41

## 6.3 Appeals Allowed

6.3.1 Of these 41 appeals (22%) were allowed by Inspectors ( i.e. the Planning Application refusal was overturned to an approval). 25 of the appeals that were allowed related to non-strategic matters which were householder /advertisement applications. These were regarding local design, character of the area or amenity or advert controls which are site and application specific judgements.

## 6.4 Appeals relating to adopted housing and employment policies.

6.4.1 The existing planning policy approach protects the suburbs from inappropriate

intensification and protects family housing from conversion to flats. It also seeks to protect existing business areas to provide jobs for local people, protect them from loss to housing, and to enable operations and buildings within them that might conflict with residential uses.

## **6.5 Housing**

6.5.1 Evidence in support of this was set out in the Protecting the Suburbs Strategy (2020) that concluded it was not practical, viable, sustainable or desirable to allow any of the family housing in the suburban residential areas to be lost to redevelopment. This is due for example to the high demand for family housing in Slough compared to the small number of new builds that are family housing, and that the majority of new builds are flats.. Further information can be seen in Protection of the Suburbs Strategy (May 2019).

6.5.2 Appeal decision results that test Core Strategy Policy CP4 (Housing Type) have shown that it is a robust and effective policy. A series of successful planning appeals won by the Council demonstrate that the policy is compliant with National Planning Policy Framework and provides the justification for the continued application of this policy in our emerging Local Plan.

6.5.3 There have been several recent planning appeal decisions on planning applications submitted on sites in the suburbs for redevelopment which the Council have won. These development proposals would have resulted in a loss of family accommodation or a negative impact on the character and appearance of the area.

6.5.4 Crucially these decisions show that, even in the absence of a five year housing land supply, the policy continues to be given weight in planning appeal decisions as PINS recognise for example the Planning issues and implications of losing family housing and poor design in Slough.

## **6.6 Employment**

6.6.1 No appeals have been received on refusal of applications to protect employment land. Employment land is critical to help meet the supply of jobs needed for Slough's young population. The various trading estates and retail parks also provide an important source of services for Slough's small business and residential communities, and business rates to support the Council's operations. Employment land is being lost through other changes of use that the Council's Local Plan has no control over (see section 4 on employment). However it is useful to know that the Policy to protect employment land has not been implicated in appeals as support for the strength of the policy where it is applied.

# **7 Natural and Built Environment**

## **7.1 Key facts**

- Slough does not have any nationally significant environmental sites but there is a need to improve the biodiversity and nature conservation value of the environmental assets that already exist in the Borough.
- Parts of Slough fall within the requirement for Burnham Beeches Special Area of Protection under the Habitat Regulations.

- Slough has some high quality parks but an overall a shortage of green infrastructure for the size of its population.
- Salt Hill Park, Pippins Park, and Herschel Park have received Green Flag Awards
- Slough has the highest concentration of data centres in Europe which will make it harder to meet our climate change targets because of their large energy consumption and associated carbon emissions.
- Slough suffers from poor air quality. There are currently 5 Air Quality Management Areas which have been declared due to breaches of the national standard. These are clustered along the A4 in the middle of Slough and around the M4/A4 in the Brands Hill area.
- Environmental issues such as the lack of the greenery, high levels of traffic congestion and limited number of appealing buildings contribute to the poor image of the town as well as having an impact on the health and wellbeing of residents.

## 7.2 Climate Change

7.2.1 The Council published and adopted a [Climate Change strategy and action plan](#) on 20 December 2021.

7.2.2 The Strategy and Action Plan has five key objectives that aim to address the causes and consequences of climate change in Slough:

1. reducing emissions from our estate and operations
2. reducing energy consumption and emissions by promoting energy efficiency measures, sustainable construction, renewable energy sources, and behaviour change
3. reducing emissions from transport by promoting sustainable transport, reducing car travel and traffic congestion, and encouraging behaviour change
4. reducing consumption of resources, increasing recycling and reducing waste
5. supporting council services, residents and businesses to adapt to the impacts of climate change

7.2.3 The council has set a target of borough-wide carbon neutrality by 2040, with an ambitious stretch target of 2030. This was outlined in [Slough's Climate Change strategy vision](#) in June 2021. This target complies with the UK's national target of net zero emissions by 2050 and a reduction of 78% of emissions by 2035 relative to 1990.

## 7.3 Burnham Beeches

7.3.1 Burnham Beeches is a Special Area of Conservation; a protected site under the Habitats Regulations (Habitats and Species Regulations 2017). Greater numbers of visitors to Burnham Beeches are causing damage to its sensitive habitat. New residential development in Slough can increase visitor numbers to the Beeches. When considering residential development planning applications the Council has a duty under the Habitats Regulations to address the impact on the Beeches.

7.3.2 A mitigation strategy can address the impact by:

- Treating selected public open spaces as 'suitable alternative natural green space' and
- implementing proposed natural habitat enhancement projects.

with the intention of attracting visitors who might have otherwise gone to Burnham Beeches

7.3.3 The enhancements would be funded by financial contributions from housing developers via Section 106 planning obligations. This would apply to residential development sites within 5.6 km of Burnham Beeches; that area covers the northern and western part of the town including the town centre.

7.3.4 An initial mitigation strategy has been agreed by Cabinet in October 2022 and it involves enhancements to Upton Court Park. A further strategy(s) is proposed to cover other open spaces such that necessary mitigation possibilities are outlined well in advance of expected residential development to which they would be linked.

7.3.5 Two advantages of having a mitigation strategy are:

- Natural England will normally object to planning applications if there is no mitigation strategy in place.
- The enhancements outlined in the strategy benefit Slough's residents and environment rather than fund projects at or around Burnham Beeches.

7.3.6 The principle of having a strategy and associated developer contributions was agreed at the June 2021 Planning Committee. The request for contributions and will be incorporated into supplementary planning guidance in the near future. And together with the strategies can form the basis of a supplementary document in the forthcoming new Local Plan.

## **7.4 Local Wildlife and Geological Sites**

7.4.1 TVERC (Thames Valley Environmental Records Centre) calculates the number of Local Wildlife Sites (LWS) and Local Geological Sites (LGS) that have been in positive conservation management in the five year period from 1st April 2018 to 31st March 2023.

Local authorities are required to report on this information to DEFRA each year as a measure of their success at protecting their local biodiversity. This reporting is known as SDL 160 (Single Data List).

7.4.2 The implementation of positive conservation management, defined as management that contributes to maintaining or enhancing the features of interest for which a site has been selected, has been assessed by Thames Valley Environmental Records Centre (TVERC) across all six Berkshire Unitary Authorities using a standardised methodology which was approved and verified by Berkshire Nature Conservation Forum (now the Berkshire Local Nature Partnership) in 2009.

7.4.3 The figure for Slough for 2023 was 71% (5 out of 7) of Local Wildlife Sites in positive management, which is the same as previous year.

## **8 Existing Planning Policy Documents**

8.1.1 Slough's Core Strategy was adopted in December 2008; Site Allocations adopted in 2010 and saved policies from the Local Plan 2004, Proposals Map 2010. There are also saved policies from the Minerals and Waste Plans. These will remain the statutory Development Plan until the new Local Plan reaches adoption.

8.1.2 More details are available on our [Local Plan page on website](#).

8.1.3 The Local Plan is accompanied by other non-development plan documents, primarily the following.

8.1.4 *Developers Guide*: a Developers Guide, originally adopted by the Council's Planning Committee for development control purposes in November 2008 was updated in 2016/17/18 in respect of affordable housing, drainage, refuse, education contributions, floorspace, viability studies, certain highway fees etc.

8.1.5 *Residential Extensions SPD*: The Residential Extensions Guidelines Supplementary Planning Document (RESPD) was adopted on 11<sup>th</sup> January 2010. The guidelines have been produced in light of increasing pressures for householders to build larger and more dominant extensions, and the impact of such extensions on both the general street scene and residential amenities within established residential areas of Slough.

## 9 Duty to Cooperate

9.1.1 The Duty to Cooperate (DtC) was created by the Localism Act 2011. It means that Slough Borough Council must engage constructively, actively and on an ongoing basis with other councils and agencies on strategic cross-boundary matters. The aim is to make sure that planning is joined up across the wider area and make local plans work well together.

9.1.2 Levelling Up and Regeneration Bill (LURB) received royal assent on 26<sup>th</sup> October, becoming an act. However, most of its provisions rely on secondary legislation and so are not yet in force.

9.1.3 At present the Council responds to consultations with adjoining neighbours regarding housing, employment, transport and statutory service and infrastructure providers such as for Habitat Regulations, water, wastewater, electricity, and health services. Table 8 shows the DTC for 22/23.

**Table 8 DTC for 2022/23**

Council	Detail	Date
Buckinghamshire	Buckinghamshire Wider Call for Sites	September 2022
West Berkshire	Local Plan review 2036 Employment Land Local Plan review- proposed Submission consultation	June 2022 January 2023
Royal Borough of Windsor and Maidenhead	Wider Growth Study Part 2- published online.	2022-2023

9.1.4 Regular meetings are held with Buckinghamshire, primarily to address the cross border housing land supply shortage, including the Wider Area Growth Study. Meetings have been held on following dates:

- 2<sup>nd</sup> November 2023

- 21<sup>st</sup> March 2023

9.1.5 A Duty to Cooperate Members and Directors meeting was held on 24<sup>th</sup> March 2023.

9.1.6 The Wider Area Growth Study was a joint study commissioned in two parts by RBWM and awarded to Stantec (formerly PBA). Part 1 was published in June 2019. The purpose of this was to define the geographic area of the study and the area of search for accommodating the future housing needs of the Slough, Windsor and Maidenhead core areas. [The report is available online.](#)

9.1.7 WAGS Part 2: potential locations for development was published in September 2022. Part 2 is about balancing need and supply. It first assesses how much land will be required to meet the development needs of the core area to 2039, over and above the supply already identified in emerging plans and their evidence bases. It then identifies and assesses potential development sites in the wider area of search that could fill that gap in supply. This search is for strategic sites, leaving out smaller-scale development opportunities. [This report is available online.](#)

## 10 Slough new Local Plan

### 10.1 Slough Local Plan

10.1.1 Slough's new Local Plan will set out a vision for the borough and the approach to development over a 20-year period. It will set targets for the delivery of different types of development, provide guidance on locations as to where this development will take place, and establish which areas should be protected. It will also set out policies by which future planning applications will be determined.

10.1.2 Progress on the plan has been consistent but impacted by matters outside of its scope such as the proposed 3rd Runway at Heathrow, and the challenges for sustainable development in accommodating demand for jobs and housing (particularly affordable and family homes) within the existing urbanised area. Funding and staff resources have also been a factor.

10.1.3 The Regulation 18 stage effectively concluded with the Preferred Spatial Strategy consulted on in November 2020, and the proposed release of selected Green Belt sites for family housing consulted on in November 2021. Reports on these were presented to Planning Committee.

10.1.4 The 5 key components of the proposed Spatial Strategy can be summarised as follows:

- **Delivering** major comprehensive redevelopment within the "Centre of Slough";
- **Selecting** other key locations for appropriate sustainable development;
- **Enhancing** our distinct suburbs, vibrant neighbourhood centres and environmental assets;
- **Protecting** the "Strategic Gap" between Slough and Greater London;
- **Promoting** the cross-border expansion of Slough to meet unmet housing needs.

### 10.2 Local Development Scheme (LDS)

10.2.1 An LDS sets out a timetable for the production of new or revised planning documents (such as a Local Plan) by the Local Planning Authority. It is effectively a programme plan for the preparation of a local plan. The Planning and Compulsory Purchase Act 2004 (as amended by the

Planning Act 2008 and Localism Act 2011) requires a Local Planning Authority to prepare and maintain a Local Development Scheme (LDS).

10.2.2 Slough’s Cabinet will review and approve an update of Slough’s LDS as part of progressing the Local Plan to Regulation 19. At present the Council are watching the evolution of changes to the Local Plan process being progressed in the Government’s Levelling Up and Regeneration Bill.

LDS Requirements:

<p>The LDS should state:</p> <ol style="list-style-type: none"> <li>the local development documents that will be produced.</li> <li>the subject matter and geographical area to which each document is to relate;</li> <li>which documents are to have ‘development plan’ status;</li> <li>which documents (if any) are to be prepared jointly with one or more other local planning authorities;</li> <li>any matter or area where there is, or is likely to be, a joint committee;</li> <li>the timetable for the preparation and revision of the documents</li> </ol>
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### 10.3 Progress on the Local Plan

10.3.1 Progress on preparation of the new Local Plan is published on the [Council's website](#).

10.3.2 The work completed so far is as follows:

- Regulation 18 Intention to Prepare a Local Plan January 2015.
- Slough Issues and Options January to February 2017
- Call for Sites January to March 2016
- Consultation on the Call for Sites June 2016
- Proposed Spatial Strategy Dec 2020 to Jan 2021
- Consultation on Proposed release of Green Belt Sites for Family Housing November 2021

## 11 Monitoring of the 2010 site allocations

	<b>Proposal Address</b>	<b>Proposed Use</b>	<b>Progress April 2023</b>
<b>SSA1</b>	Lynch Hill and Bangle's Spinney	Non-statutory informal nature reserve	Some wildlife management works underway.
<b>SSA2</b>	Britwell and Haymill Regeneration Area	Mixed use: community, retail, residential and public open space	Complete
<b>SSA3</b>	Newbeech, Long Readings Lane, Elderly Persons	Residential (family housing) and or community use	Complete 12/13

Proposal Address		Proposed Use	Progress April 2023
	Home and day centre		
<b>SSA4</b>	Slough Trading Estate (including Leigh Road Central Core Area)	Mixed use: Offices, Research and Development, Light Industrial, General Industrial, Storage and Distribution, Residential, Retail, Food and Drink, Hotels, Conference Facilities, Educational Facilities, Recreation and Leisure Uses.	Outline planning permission granted 18/06/12. Application lapsed
<b>SSA5</b>	149-153 Farnham Road and 415-426 Montrose Avenue	Retail (extension or redevelopment of existing supermarket with car parking)	Complete
<b>SSA6</b>	352-358 Farnham Road	Retail (extension to supermarket)	Complete in 2011/12
<b>SSA7</b>	Cippenham Phase 4	Residential (family housing)	Complete 2012/13
<b>SSA8</b>	Watercress Beds West of Keel Drive	Non-statutory informal nature reserve	Some tidying up and access work carried out.
<b>SSA9</b>	Thames Valley Community Centre	Mixed use: community and education	Complete. New school, community hub and nursery school on expanded site
<b>SSA10</b>	Chalvey Millennium Green	Non-statutory informal nature reserve	None. Trust land now under Council control. Enhancements proposed as part of adjacent Montem site residential development.
<b>SSA11</b>	Slough Town Hall	Mixed use: residential, community, education, commercial, non-residential institution	Completed school and residential complete.
<b>SSA12</b>	Land South of Stranraer Gardens	Non-statutory informal nature reserve	None
<b>SSA13</b>	Heart of Slough	Comprehensive regeneration for residential, offices, hotel, bus station, library, retail,	Transport improvements completed and bus station complete 11/12. Library complete. One office building



Proposal Address		Proposed Use	Progress April 2023
		restaurants and cafes, drinking establishments, education, leisure, associated changes to the road network, improvements to the public realm and parking.	complete, a second one has permission. Hotel and residential on former Library site complete. Main site (former university/NW Quadrant) demolished; Pre app discussions held for redevelopment.
<b>SSA14</b>	Queensmere and Observatory Shopping Centres	Mixed use: retail, leisure, residential	Outline application agreed by Planning Committee for Queensmere site.
<b>SSA15</b>	Upton Hospital, Albert Street	Medical and Healthcare uses	Occasional discussions with NHS. Likely to include some new residential alongside new/retained health care facilities.
<b>SSA16</b>	Post Office Sorting Office, Wellington Street	or mixed use: business and residential	No progress to date
<b>SSA17</b>	Slough Canal Basin, Stoke Road	Mixed use	Planning Application agreed by Planning Committee (granted permission post April 23). Council likely to sell its part of site.
<b>SSA18</b>	Former Arbour Vale School, West Wing, St. Joseph's playing field, Stoke Road	– Development of secondary school and community sports stadium	Complete
<b>SSA19</b>	Play Area off Moray Drive	residential development	Complete
<b>SSA20</b>	Wexham Park Hospital, Wexham Road		Partial redevelopment scheme complete. Residential element may be introduced.
<b>SSA21</b>	Halkingcroft Wood, Middlegreen Road		No progress

	<b>Proposal Address</b>	<b>Proposed Use</b>	<b>Progress April 2023</b>
<b>SSA22</b>	BT Site and 297 Langley Rd	Residential	Complete
<b>SSA23</b>	Part of Langley Business Centre, 11/49 Station Rd, Langley	Retail	Planning permission granted for data centre and some limited residential. Existing buildings being demolished.
<b>SSA24</b>	Land west of Hollow Hill Lane, Langley		No progress. Most of site needed for proposed Western Rail Link to Heathrow – rail infrastructure plus flood mitigation land.
<b>SSA25</b>	Old Slade Lake, Orlits Lake and Colnbrook west, Lakeside Road, Colnbrook		No progress. Site affected by Heathrow third runway proposal.

## 12 Selected Key Locations

12.1.1 Core Policy 1 (Spatial Strategy) provides for relaxation of Core Policy land use policy (existing business area or shopping areas) if certain objectives are met. The Site Allocations DPD identified 4 areas where this policy could apply. Progress on implementation is below:

- SKL1 Sites at Bath Rd Cippenham (relaxation of loss of existing business area)  
2 planning applications for primarily residential development being considered. Other sites remain in business/retail use)
- SKL2 Chalvey High St (Ladbroke Rd) (relaxation of loss of existing shopping area)  
Site redeveloped for education and community uses combined with adjacent site SSA 9 (former community centre)
- SKL3 Stoke Road and Mill St (relaxation of loss of existing business area)  
Several residential redevelopment or change of use schemes have been completed. Developer interest in redevelopment of 3 more sites.
- SKL4 Former Langley Oil Terminal/Railway Station car Park off Station Rd (relaxation of loss of existing business area)

Most of the site remains in employment uses – open yard uses.

12.1.2 Details of each of the Selected key locations (SKL) can be found in this document here [www.slough.gov.uk/downloads/file/2274/development-plan-site-allocations](http://www.slough.gov.uk/downloads/file/2274/development-plan-site-allocations)

## 13 Minerals and Waste

13.1.1 Slough's view is at present that planning on minerals and waste issues can be addressed through the existing saved policies, NPPF, and Preferred Areas and liaison with the Berkshire and other Authorities on their strategies.

13.1.2 We have decided not to prepare a new Minerals Plan for Slough at this stage because there are very few remaining sites for gravel extraction left in the Borough. We have also decided not to prepare a Waste Plan as we have sufficient capacity for Slough with the Energy from Waste plant in Colnbrook

13.1.3 The Council is also a member of the South East Waste Planning Advisory Group (SEWPAG). That monitors sub-regional issues including potential impacts from London.

13.1.4 The National Planning Policy Framework was published in March 2012, updated in 2018, 2019 and 2021. This Council has incorporated Saved Minerals Local Plan policies into a Composite Local Plan published in July 2013. As a result the adopted Minerals Plan remains the Berkshire of 1995 (revised in 1997 and 2001) and the Waste Plan Minerals remains the Berkshire Plan of 1998.

13.1.5 Slough BC has historically had its extensive mineral extraction on green belt land. These have now been largely restored. The remaining parts of Slough are built up. There are no further large-scale reserves available in Slough. As a result Slough is at less risk compared with elsewhere in Berkshire to meet future needs of the region. In Waste Planning terms, Slough BC has a relatively small percentage of Municipal Solid Waste going to landfill and conversely a relatively high proportion of MSW going to Energy from Waste.

## Appendices

Table A Completions of new homes 2022/23 summary

Table B Summary of hard commitments (those with planning consent) for residential development as at 1/4/23

Table C Core Strategy Plan period to 2023 residential net completions compared to target/need

Table D Housing trajectory new Local Plan period

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## Table A Completions of new homes 2022/2023 summary

ward	reference	address	development description	construction status (site)	total gross units (for site)	gross completed 22/23	net completed 22/23	loses	Change of Use (cou) or conv and permitted development (pd) 22/23
Central	P/01508/042	Aspire 2 Site, Corner of Church Street and Herschel Street, Slough, SL1 1PG	Construction of a part eight and part nine storey building (Class C3 Use) to accommodate 238 flats together with 47 on site car park spaces at grade together with landscaping and other works incidental to the development.	under construction	238	0	0	0	
Central	P/06865/012	Land rear of 9-15, High Street, Slough, Berkshire, SL1 1DY	Construction of a 6 storey building to provide 8no. 1 bed & 6no. 2 bed flats with 6no. undercroft parking spaces.	complete	8	8	8	0	
Central	P/16250/000	1a, St. Pauls Avenue, Slough, SL2 5EX	Construction of 8 no. X 2 bed flats contained within one three storey building and one part three storey/part two and half storeys building and 1 no. detached three bedroom house together with car parking and landscaping.	under construction	9	0	0	0	
Central	P/01456/006	24-26, Albert Street, Slough, SL1 2BU	Change of use of existing buildings from C1 Guest Houses to Sui Generis Large Homes in Multiple Occupation	complete	1	1	1	0	cou
Chalvey	F/10427/010	7, Bath Road, Slough, SL1 3UA	Prior approval application for the change of use of office (Use Class B1a) to residential (Use Class C3) to create 67 residential dwellings.	complete	67	67	67	0	cou pd
Chalvey	P/09314/004	39-41, Ledgers Road, Slough, SL1 2RQ	Proposed construction of 2no dormer windows on the rear elevation and conversion of loftspace to create a 1no studio apartment	complete	1	1	1	0	
Chalvey	Y/10427/011	7, Bath Road, Slough, SL1 3UA	Prior Approval for the addition of 2 storeys of residential development (use class C3) above an existing office (Use Class B1a) under Class AA, Part 3, Schedule 2 of the Town and Country Planning (General Permitted Development) (Amendment) (England) Order 2015	complete	19	19	19	0	pd
Chalvey	P/19227/001	190, Chalvey Grove, Slough, SL1 2TE	Construction of a 1no 3 bedroom detached dwelling.	complete	1	1	1	0	
Cippenham Green	P/00611/006	Cippenham Working Mens Club, 505-507, Bath Road, Slough, SL1 6AA	Construction of 5no new semi-detached & terraced houses following the demolition of existing working mens club	complete	5	5	5	0	
Cippenham Meadows	P/01766/025	172-184, Bath Road, Slough, Berkshire, SL1 3XE	Demolition of existing building. Construction of a six storey building to accommodate 28no. 3 bed flats with undercroft parking.	complete	28	28	28	0	
Cippenham Meadows	P/17694/000	19, Cress Road, Slough, SL1 2XT	Erection of single storey rear extension to number 19 Cress Road. Erection of two storey side extension to number 19 Cress Road to create 1no. three bedroom dwelling and associated amenity space, access and parking.	complete	1	1	1	0	

ward	reference	address	development description	construction status (site)	total gross units (for site)	gross completed 22/23	net completed 22/23	loses	Change of Use (cou) or conv and permitted development (pd) 22/23
Colnbrook with Poyle	P/06910/003	8A, Cherwell Close, Slough, SL3 8XB	Construction of a 1no 3 bedroom dwelling at the rear of 8a Cherwell Close and associated boundary and landscape works	complete	1	1	1	0	
Elliman	P/00094/039	Horlicks Factory, Stoke Poges Lane, Slough, SL1 3NW	A hybrid planning application for part outline/part detailed planning permission for a residential-led mixed use development of up to 1,300 (now 1,277 ) new homes comprising: 1. Outline planning permission for the provision of up to 746 new homes (Use Class C3), commercial (Use Class A1-A3) and ancillary facilities within new buildings extending up to 77m AOD in height; car and cycle parking; public realm, landscaping and amenity space and all associated works (with all matters reserved); (subsequently revised to 701 homes under RM P/00094/068 2/09/2022 Submission of Reserved Matters Application (Access, Layout, Scale, Design and Landscaping) for Blocks C, F, H J and N of the Horlicks Factory site, comprising 701 residential units, commercial floorspace, associated landscaping and amenity spaces, parking, access routes and associated works ) and 2. Full planning permission for the part demolition of the existing Horlicks Factory, and demolition of factory outbuildings and structures, to facilitate the erection of five new buildings ranging from one storey to 10 storeys including change of use, two storey rooftop extension, ground floor extensions and alterations to the remaining parts of the Factory, to provide 554 new homes (Use Class C3), upto 239sqm commercial floorspace (Use Classes A1-A3) and a nursery (Use Class D1), and ancillary facilities; relocation of the war memorial; car and cycle parking; public realm, landscaping and amenity space; access from Ploughlees Lane, Stoke Poges Lane and Stokes Gardens; and all associated works. (subsequently revised to 576 homes under P/00094/052 13/1/21).	under construction	1277	152	152	0	
Elliman	P/02367/005	207 B, Stoke Road, Slough, Slough, SL2 5AX	Lawful development certificate for the existing use of a self contained outbuilding which has been in place for more than 4 years	complete	1	1	1	0	
Elliman	P/07208/001	69B, Stoke Road, Slough, SL2 5BJ	Lawful development certificate for existing use of property as two separate flats	complete	2	2	1	1	conv
Farnham	P/11212/006	14, Westfield Road, Slough, SL2 1HE	Conversion of a 6 bedroom dwelling to create two separate dwellings comprising of 1no. 2 bedroom dwelling and 1no. 4 bedroom dwelling.	complete	2	2	1	1	conv
Farnham	P/19400/000	Land adj to, 97, Lancaster Avenue, Slough, SL2 1AU	Construction of 1no 3 bedroom detached dwelling.	complete	1	1	1	0	

ward	reference	address	development description	construction status (site)	total gross units (for site)	gross completed 22/23	net completed 22/23	loses	Change of Use (cou) or conv and permitted development (pd) 22/23
Haymill & Lynch Hill	P/19735/002	7, Royston Way, Slough, SL1 6EP	Retrospective application for partial demolition of existing bungalow and construction of a single dwelling house	complete	1	1	0	1	
Haymill & Lynch Hill	P/06147/005	88, Lynch Hill Lane, Slough, SL2 2QB	Change of use from C2 (residential institutions) to C3 (dwellinghouse)	complete	1	1	1	0	cou
Langley St. Marys	P/17249/002	77, Harrow Road, Slough, SL3 8SH	Construction of a part single, part double storey rear extension to no.77 Harrow Road and construction of 1no 3 bedroom house adjacent to no 77 Harrow Road	complete	2	1	1	0	
Upton	F/01308/030	Churchill House, 1, London Road, Slough, SL3 7RL	Prior approval for change of use from office (use Class B1a) to residential (Use Class C3) to create 28 residential dwellings.	complete	28	28	28	0	cou pd
Upton	P/01308/031	Churchill House, 1, London Road, Slough, SL3 7FJ	Construction of 2no dwellings through the enclosure/infilling of existing undercroft area on the east side of the ground floor of Churchill House, 1 London Road	complete	2	2	2	0	
Upton	P/01308/034	Churchill House, 1, London Road, Slough, SL3 7FJ	Roof alterations to create a single dwelling within the existing attic space at Churchill House, 1 London Road, Slough, SL3 7FJ	complete	1	1	1	0	
Wexham Lea	P/09691/003	2, Berryfield, Slough, SL2 5SE	Construction of 1no 2 bedroom attached dwelling.	complete	1	1	1	0	
			totals			325	322	3	

**Table B Summary of hard commitments (those with planning consent) for residential development as at 1/4/23.**

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Baylis & Stoke	P/00671/018	93, Stoke Poges Lane, Slough, SL1 3NJ	Demolition of the existing restaurant and the construction of a 2no. four storey buildings connected by podium amenity space at first floor level to provide 43 no. residential apartments (11 x 1 bed; 30 x 2 bed; 2 x 3 bed). Undercroft car parking, new access/egress and associated works.	UC	46	0	0	46	0	
Baylis & Stoke	P/01031/006	St Michael & All Angels Church, Whitby Road, Slough, SL1 3DW	Demolition of existing church, hall and vicarage. Erection of three pairs of semi detached properties and one detached dwelling with associated parking, landscaping and alterations to the existing access road.	NS	6	0	0	0	6	Most unlikely to progress
Britwell & Northborough	P/09764/001	12, Gascons Grove, Slough, SL2 1TH	Construction of a 3-bedroom dwelling	NS	1	0	0	0	1	
Britwell & Northborough	P/14896/005	143, Farnham Lane, Slough, SL2 2AS	Construction of 1no three bedroom dwelling	UC	1	0	0	1	0	
Britwell & Northborough	P/01006/011	279 , Long Furlong Drive, Slough, SL2 2LY	Construction of a first floor to create a self-contained 3 bedroom flat with associated parking.	NS	1	0	0	0	1	
Britwell & Northborough	P/19765/000	181, Wentworth Avenue, Slough, SL2 2AW	Construction of 1no new dwelling and a new garage, facilitated by the demolition of the existing garage, on the land of 181 Wentworth Avenue (amended description).	UC	1	0	0	1	0	
Britwell & Northborough	P/15362/004	243, Long Furlong Drive, Slough, SL2 2LY	Construction of 1 x 3 bedroom dwelling attached to the main house with associated car parking to the rear	UC	1	0	0	1	0	



ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Britwell & Northborough	P/15464/000	80, Doddsfield Road, Slough, SL2 2AH	ERECTION OF A TWO BEDROOMED DETACHED DWELLING.	UC	1	0	0	1	0	
Central	F/10913/019	My Council, Landmark Place, High Street, Slough, SL1 1JL	Prior approval for change of use from Class B1(a) offices to Class C3 to create 89 residential units, ranging from studios, 1 bed and 2 bed units.	UC	89	0	0	89	0	
Central	F/10913/020	My Council, Landmark Place, High Street, Slough, SL1 1JL	Prior approval notification for a change of use from commercial, business and service (Class E) to residential (class C3) (8 flats (2x 2 bedroom and 6 x 1 bedroom).	UC	8	0	0	8	0	
Central	F/10913/027	My Council, Landmark Place, High Street, Slough, SL1 1JL	Prior approval notification for a change of use from commercial, business and service (Class E) to residential (Class C3) (9 flats (1x 2 bedroom, 6x 1 bedroom and 2x studios)	UC	9	0	0	9	0	
Central	P/00072/096	Akzonobel Decorative Paints, Wexham Road, Slough, SL2 5DB	Outline planning application (to include matter of principal points of access), to be implemented in phases, for mixed use development comprising: a) Demolition of existing buildings and structures and preparatory works (including remediation) and access from Wexham Road; b) up to 1,000 residential dwellings (Use Class C3); along with flexible commercial uses including all or some of the following use classes A1 (Shops), A2 (Financial and Professional Services), A3 (Food and Drink), D1 (Non-residential Institutions) and D2 (Assembly and Leisure); car parking; new public spaces, landscaping; vehicular and pedestrian access; and c) the provision of commercial floorspace including all or some of the following use classes B2 (General Industry), B8 (Storage or Distribution) and sui generis data centre (including ancillary office space and associated plant and infrastructure provision); car parking, landscaping and vehicular and pedestrian access. (Matters of Scale, Layout, Appearance, and Landscaping to be dealt with by reserved matters).	NS	1000	0	0	0	1000	Outline PP. And most unlikely to progress.

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Central	P/00475/011	Spring Cottages, Upton Park, Slough, Berks, SL1 2DH	Construction of 4no. 4 bed, two storey detached houses with associated car parking and landscaping. Access taken from Upton Park.	UC	4	0	0	4	0	
Central	P/01276/003	279, High Street, Slough, Berkshire, SL1 1BN	Outline Planning Permission (Access, Layout, Scale) for the demolition of the existing buildings on 277-279 High Street and redevelopment consisting three buildings, one of part five/six storey, one seven storey and a single storey link building to provide 65no. residential flats (6 no.studio flats, 9no. 1 bed flats, and 50 o. 2 bed flats), 2 retail/commercial units, cycle storage facility and car parking for 21no. cars.	NS	65	0	-1	0	65	Outline PP
Central	P/01508/042	Aspire 2 Site, Corner of Church Street and Herschel Street, Slough, SL1 1PG	Construction of a part eight and part nine storey building (Class C3 Use) to accommodate 238 flats together with 47 on site car park spaces at grade together with landscaping and other works incidental to the development.	UC	238	87	87	151	0	
Central	P/01571/012	15-23, Church Street, Slough, Berkshire, SL1 2NL	Demolition of existing building at 15-23 Church Street. Construction of a detached six storey building comprising retail, parking, and bin store at ground floor, and 41 residential flats to the upper floors (13no. 2 bed; 13no. 1 bed; 15no. studios).	NS	41	0	0	0	41	
Central	P/01601/023	Burger King (UK) Ltd, 150-152, High Street, Slough, SL1 1JP	Construction of a four storey building to provide Class E commercial use at ground	UC	8	0	0	8	0	
Central	P/01645/013	115, High Street, Slough, SL1 1DH	Construction of a three-storey rear extension(1st, 2nd, 3rd floor), roof alterations and enlargement, Conversion of 3no. Bedroom house into 1no. 1 bedroom flat and 2no. 2 bedroom flats with the provision of private amenity area above ground floor unit, insertion of roof lights and associated works.	UC	3	0	-1	3	0	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Central	P/02278/018	WESTMINSTER HOUSE, 31-37, WINDSOR ROAD, SLOUGH, BERKS., SL1 2EL	Construction of a 5 storey building to provide 17 no. flats on land to the rear of 31-37 Windsor Road together with 6 no. car parking spaces.	UC	17	0	0	17	0	
Central	P/02418/038	234-236, High Street, Slough, Berkshire, SL1 1JU	Conversion of existing first floor ancillary retail storage to residential flats (C3 Use Class) and upward extension over ground floor and first floor to create 14 flats (4 x studio flats; 8 x 1 bed flats; 2 x 2 bed). Rear extension for secure cycle store and bin store. Photovoltaic panels on flat roof. Balconies at rear, terrace at front.	NS	1	0	0	0	1	
Central	P/02586/001	Land R/O, 7, Windsor Road, Slough, SL1 1JL	Construction of a 9 storey building with basement to accommodate 24no. flats (2 bedrooms) with associated bin and cycle storage and 3 no. parking spaces to be retained and annex office (B1A) on ground floor at rear and associated works.	UC	24	0	0	24	0	
Central	P/02649/002	98, Park Street, Slough, SL1 1PU	Demolition of existing building and construction of a new 3 storey building comprising of 3no 1 bedroom flats and 3no 2 bedroom flats.	NS	6	0	-3	0	6	
Central	P/02683/013	204-206, High Street, Slough, SL1 1JS	Demolition and Redevelopment of the existing site for a mixed use development comprising replacement flexible retail space (Class A1,A2,A3 uses) at ground floor level, flexible commercial floorspace at first floor fronting the High Street for either B1 (offices) or Class D1 (gym) uses and 94 residential dwellings within 3 buildings at podium level across the site with heights of 5, 14 and 5 storeys. Shared amenity space provided at first floor podium level, with cycle, waste and recycling storage facilities at ground floor level (revised by P/002683/015 to 89 units)	UC	89	0	0	89	0	
Central	P/03138/014	10, The Grove, Slough, SL1 1QP	Construction of an upward extension, external alterations and change of use of the existing office building to create 11no. self-contained flats and the erection of 5no. new dwellings to the rear, including landscaping and the provision of secure cycle and bin storage	UC	16	0	0	16	0	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Central	P/03748/014	127 High Street, Slough, SL1 1DH	Change of use from offices to residential (class c3) including loft conversion.	UC	5	0	0	5	0	
Central	P/04393/013	17-19, Albert Street, Slough, Berkshire, SL1 2BE	Demolition of existing building and redevelopment of the site to provide with a two storey building to accommodate a retail unit at ground floor and 7 x 1 bed flats.	NS	7	0	0	0	7	
Central	P/04730/011	The Rose And Crown, 312, High Street, Slough, SL1 1NB	Conversion of existing first floor into a self contained two bedroom flat and retention of ground floor area as a Pub with minor layout alterations. Creation of a new dedicated access point from Hatfield Road to the new first floor flat. Creation of a passage above the existing rear extension to include the erection of a new external staircase. New door and high-level window at first floor in rear elevation to service the new residential unit.	NS	1	0	0	0	1	
Central	P/04829/028	The Village, 102-110, High Street, Slough, SL1 1HL	Change of use of second floor from former day care centre into 7no residential units	NS	7	0	0	0	7	
Central	P/05806/007	23-25, Mill Street, Slough, Berkshire, SL2 5AD	Demolition of the existing building and redevelopment of the site to provide two connected residential blocks for 3 x 3 bedroom, 32 x 2 bedroom and 21 x 1 bedroom units, associated parking and cycle parking facilities; with a new public pedestrian and cycle route connecting Fleetwood Road and Mill Street.	UC	56	0	0	56	0	
Central	P/08105/013	18-20, Park Street, Slough, SL1 1PD	Erection of two storey extension above first floor level offices including full height side extension to create 6 x self contained residential flats to provide 2x studios, 2x one bedroom apartments and 2x two bedroom self-contained units, with balconies, provision of bin and cycle stores, internal and external alterations to the ground floor shop/cafe and first floor offices and other associated works.	NS	6	0	0	0	6	
Central	P/13542/015	100A, Wexham Road, Slough, Berkshire, SL2 5EJ	Conversion of existing tyre shop into 2no. 3 bed houses. Infill existing openings and addition of new windows.	UC	2	0	0	2	0	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Central	P/16250/000	1a, St. Pauls Avenue, Slough, SL2 5EX	Construction of 8 no. X 2 bed flats contained within one three storey building and one part three storey/part two and half storeys building and 1 no. detached three bedroom house together with car parking and landscaping.	UC	9	7	7	2	0	
Central	P/19443/000	30-32, Wexham Road, Slough, SL1 1UA	Demolition of the existing buildings and redevelopment of the site at 30-32 Wexham Road, Slough, SL1 1UA to create 18 new residential units with associated parking and landscaping.	NS	18	0	-1	0	18	
Central	P/02411/022	Thames Central, Hatfield Road, Slough, SL1 1QE	Construction of a side and roof extension to existing building to provide 52 apartments, involving the formation of a fourteen storey building.	NS	52	0	0	0	52	
Central	P/03079/017	190-192, High Street, Slough, SL1 1JS	Redevelopment of the site to provide a part six, part eight storey building to form 61 residential units (Use Class C3); re-provision of 2 commercial units (Use Class E); associated cycle parking, refuse storage; roof garden; new residential access to the front (north) elevation; and upgrades to the high street façade.	NS	61	0	0	0	61	
Central	P/00731/042	26-40, Vanburgh Court, Stoke Road, Slough, SL2 5YG	Construction of a 1no 3 bedroom flat and the demolition and relocation of the existing bike storage area at the ground floor and reorganisation of cycle storage at ground floor level.	NS	1	0	0	0	1	
Central	P/07171/005	2a, Alpha Street North, Slough, SL1 1RB	Construction of an upward extension to create a second floor, together with the conversion of the existing first floor, to create 2 x 1 bed self-contained flats, and associated alterations including rear external staircase with privacy screens and provision of bin storage and cycle parking	NS	2	0	0	0	2	
Central	P/07698/003	31, Wellesley Road, Slough, Slough, SL1 1UX	Construction of 1no new 2 bedroom dwelling.	NS	1	0	0	0	1	
Central	P/08802/010	Merton Court, 4, Merton Road, Slough, SL1 1QR	Construction of a new second floor and roof to existing apartment block at Merton Court to create 4 new self-contained flats with additional parking space with bicycle and bin storage.	NS	4	0	0	0	4	

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Central	F/01190/030	210-216, High Street, Slough, SL1 1JS	Prior approval notification for 2 additional floors to provide 10 self-contained residential units.	NS	10	0	0	0	10	
Central	P/02465/013 P/02465/014	226-228, High Street, Slough, Berkshire, SL1 1JS	Construction of four storey detached building to accommodate retail (Class A1) to the front end at ground floor level, and residential flats/ studio apartments above, (1 No. 2 bed flat; 6 No. One bed flats; 7 No Studio apartments). Bin store and cycle parking within the rear end of the ground floor (P2465/13 226). Construction of a 4no. storey A1 retail at Ground Floor and 3no. floors of C3 residential to provide 14no. residential apartments (P/2465/014 228).	UC	12	0	0	12	0	
Chalvey	F/01289/018	108, Windsor Road, Slough, SL1 2JA	Prior approval for change of use from Class E offices to Class C3 to create 2 flats (2x 2 bedroom) on the first and second floors of the building.	NS	2	0	0	0	2	
Chalvey	P/00114/008	Garages Rear Of 1, Alexandra Road, Slough, SL1 2NQ	Demolition of existing garages and construction of 1no. two bedroom and 2no. one bedroom flats	NS	2	0	0	0	2	
Chalvey	P/00322/023	Greenwatt Way, Primary Road, Slough, SL1 2ES	Reserved matters application (for external appearance) following approval of outline planning permission P/00322/019 for extra care flats.	NS	60	0	0	0	60	
Chalvey	P/01219/004	Land Rear Of, 14-20, Chalvey Road East, Slough, Berkshire, SL1 2LU	Demolition of a former workshop to the rear with corrugated iron roof used for storage, and a brick structure adjacent to the front boundary to facilitate the construction of a proposed new detached house (amended description)	NS	1	0	0	0	1	
Chalvey	P/01621/014	25-31, Hillside, Slough, Berkshire, SL1 2RW	Refurbishment of 2 no. existing houses and construction of 2 no. 2 bed homes, and 4 no. 3 bedroom detached houses.	UC	6	0	0	6	0	

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Chalvey	P/06033/045	15, Bath Road, Slough, SL1 3GJ	Extensions to the existing building to provide two residential units, alterations and additions to existing dwellings.	UC	2	0	0	2	0	
Chalvey	P/16841/000	Slough Family Centre, Chalvey Park, Slough, SL1 2HX	Construction of 4no. four bedroom houses and 6no.three bedroom houses.	NS	10	0	0	0	10	
Chalvey	P/16995/000	12-20, Windsor Road, Slough, SL1 2EJ	Demolition of existing buildings (including the locally listed building at 18-20 Windsor Road). Construction of a part 3 storey, part 4 storey, part 5 storey detached building to accommodate 3 no. retail units at ground floor fronting the Windsor Road; 2no. 1 bed flats to the rear of the ground floor; and 20no. residential flats to the upper floors flats to the upper floors. The building in total would comprise 8no. 2 bed flats; 14no. 1 bed flats; 3no. retail units (total retail 206.1 square metres).	NS	22	0	0	0	22	
Chalvey	P/00669/024	Forward Building, 44-46, Windsor Road, Slough, SL1 2EJ	Change of use of the existing Class E commercial uses on the ground floor to provide three dwellings (Use Class C3)	NS	3	0	0	0	3	
Chalvey	P/12595/005	27-29, Chalvey Road West, Slough, SL1 2NF	Change of use of ground floor offices to residential studio apartments	NS	2	0	0	0	2	
Chalvey	P/07383/010	Former leisure centre site, car park, streamside area and woodland, Montem Lane, Slough	Full planning permission for residential development (Use Class C3); car and cycle parking; public realm, landscaping works and amenity space; access from Montem Lane; and all ancillary works and infrastructure. Including enhancements to woodland and streamside area.	NS	212	0	0	0	212	

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Chalvey	P/06877/004	2-20 & 22-48, Hartland Close, Slough, SL1 3XT	Alteration and refurbishment of 23no. existing single occupancy studio flats and a three-bedroom landlord's flat & office to be reconfigured into 15no. 1 bedroom flats; enlargement, replacement and infill of uPVC windows & doors to elevations, in anticipation of internal remodelling; and, replacement of existing single skin external refuse facilities into proposed bicycle and general storage.	NS	15	0	-9	0	15	
Chalvey	P/13474/004	Land R/O, 110-122 The Crescent, Slough, SL1 2LG	ERECTION OF 2 NOS. 4 BED DETACHED DWELLINGS, ASSOCIATED 1 NO. OUTBUILDING AND 1 NO. HOME OFFICE. INCLUDING DETAILS OF LANDSCAPING, SECURED BIN STORAGE, SECURED CYCLE PARKING, SAMPLES OF MATERIALS, DRAINAGE AND CONTAMINATION.	NS	2	0	0	0	2	
Chalvey	P/19641/000	23, Montem Lane, Slough, SL1 2QW	Lawful development certificate to establish the use of 23 Montem Lane to be converted in to 2no flats more than 10 years ago	NS	2	0	0	0	2	
Cippenham Green	F/02069/017	337 Bath Road, Slough, SL1 5PR	Prior approval for change of use from class B1 (A) offices to class C3 residential for 13no residential dwellings	UC	13	0	0	13	0	
Cippenham Green	P/03026/003	399-401, Bath Road, Slough, SL1 5QL	Replacement of existing shops with new shop and flat	NS	1	0	0	0	1	Overlap. Subsequent PP
Cippenham Green	P/03026/005	399-401, Bath Road, Slough, SL1 5QL	Construction of a part single storey, part two storey rear extension to 399 Bath Road, new two storey attached building to create 1no. retail unit and 1no. 2 bed flat on ground floor and 1no. 2 bed flat on first floor following demolition of 399a and 401 Bath Road.	NS	2	0	0	0	2	
Cippenham Green	P/10822/004	8, Moreton Way, Slough, SL1 5LT	Construction of a 4no. bedroom house, with a rear dormer to facilitate loft accommodation, associated parking and amenity space.(Retrospective)	NS	1	0	0	0	1	



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Cippenham Green	P/18001/000	Garage Site adjacent to, 20, Mallard Drive, Cippenham, Slough	Construction of 3 x two bedroom houses with parking and amenity space	NS	3	0	0	0	3	
Cippenham Green	P/06775/005	2A, Ivy Crescent, Slough, SL1 5DA	Construction of a 1no. 3 bedroom detached dwelling	NS	1	0	0	0	1	
Cippenham Green	P/07105/003	24, Mallard Drive, Slough, SL1 5BP	Construction of a 1 x 3 bedroom dwelling next to 24 Mallard Drive	NS	1	0	0	0	1	
Cippenham Green	P/16938/001	3A, Huntercombe Lane South, Maidenhead, SL6 0PQ	Construction of a replacement detached dwelling	NS	1	0	0	0	1	
Cippenham Meadows	F/11168/012	Arvato, Phoenix One, 59-63 Farnham Road, Slough, SL1 3TN	Prior approval for change of use from Class B1(a) offices to Class C3 to create 56 residential dwellings (1 and 2 bedroom units)	NS	56	0	0	0	56	
Cippenham Meadows	P/04241/014	163, Bath Road, Slough, SL1 4AA	Change of use from serviced apartments (C1 use class) to self-contained flats and extension	UC	90	0	0	90	0	
Cippenham Meadows	P/06954/030	Atria House, 219, Bath Road, Slough, SL1 4AA	Construction a four storey detached building to accommodate 37 self contained flats (10no. Studios; 16no. 1 bed flats; and 11no. 2 bed flats). Associated parking, communal areas, and landscaping. Access taken from Bath Road.	NS	37	0	0	0	37	

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Colnbrook & Poyle	P/09960/016	Ye Olde George Inn, 146, High Street, Colnbrook, Slough, SL3 0LX	Construction of 2no. three bedroom semi-detached dwellings, landscaping and associated works on the land to the rear of Ye Olde George Public House	NS	2	0	0	0	2	
Colnbrook & Poyle	P/11009/012	559, London Road, Slough, SL3 8QE	Construction of a 1no. detached bungalow with associated parking, amenity space and installation of vehicular cross over	NS	1	0	0	0	1	
Colnbrook & Poyle	P/14825/008	Star & Garter, Park Street, Colnbrook, Slough, SL3 0JF	Demolition of existing building (The Smithy) and construction detached building comprising a cafe at ground floor with external seating (A3 Use Class); and 6no. residential flats (C3 Use Class) above within the first floor and roof void (4no. 2 bed; 2no. 1 bed). Associated car parking and landscaping. Demolition of the single storey elements of the vacant public house and construction of a single storey rear extension with other minor external alterations and conversion to form 3no. residential flats (2no. 1 bed; 1no. 1 bed).	NS	9	0	0	0	9	
Colnbrook & Poyle	P/16609/006	28, Springfield Road, Slough, SL3 8QJ	Retrospective planning application for the demolition of existing bungalow and construction of a single dwelling house with an ancillary outbuilding to the rear of the garden	UC	1	0	-1	1	0	
Colnbrook & Poyle	P/12033/002	Dulce Domum, Bath Road, Colnbrook, Slough, SL3 0HZ	Demolition of existing bungalow and construction of 2no dwelling and 2no dropped kerb accesses (AMENDED PLANS)	NS	2	0	0	0	2	

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Elliman	P/00094/039	Horlicks Factory, Stoke Poges Lane, Slough, SL1 3NW	A hybrid planning application for part outline/part detailed planning permission for a residential-led mixed use development of up to 1,300 ( <i>now 1,277</i> ) new homes comprising: 1. Outline planning permission for the provision of up to 746 new homes (Use Class C3), commercial (Use Class A1-A3) and ancillary facilities within new buildings extending up to 77m AOD in height; car and cycle parking; public realm, landscaping and amenity space and all associated works (with all matters reserved); ( <i>subsequently revised to 701 homes under RM P/00094/068 2/09/2022 Submission of Reserved Matters Application (Access, Layout, Scale, Design and Landscaping) for Blocks C, F, H J and N of the Horlicks Factory site, comprising 701 residential units, commercial floorspace, associated landscaping and amenity spaces, parking, access routes and associated works</i> ) and 2. Full planning permission for the part demolition of the existing Horlicks Factory, and demolition of factory outbuildings and structures, to facilitate the erection of five new buildings ranging from one storey to 10 storeys including change of use, two storey rooftop extension, ground floor extensions and alterations to the remaining parts of the Factory, to provide 554 new homes (Use Class C3), up to 239sqm commercial floorspace (Use Classes A1-A3) and a nursery (Use Class D1), and ancillary facilities; relocation of the war memorial; car and cycle parking; public realm, landscaping and amenity space; access from Ploughlees Lane, Stoke Poges Lane and Stokes Gardens; and all associated works. ( <i>subsequently revised to 576 homes under P/00094/052 13/1/21</i> ).	UC	1277	516	516	312	701	
Elliman	P/01120/024	93A, Stoke Road, Slough, SL2 5BJ	Alterations and change of use of existing 3no. HMO rooms to a 2 bedroom flat on first floor and change of use of existing 2 bedroom flat at basement level to office B1a.	NS	1	0	-1	0	1	
Elliman	P/05597/015	10, Stoke Gardens, Slough, SL1 3QQ	Construction of two additional floors creating a third and fourth floor comprising 5no. residential flats (4no. two bedroom and 1no. one bedroom flats with parking) with existing basement level car park.	UC	5	0	0	5	0	

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Elliman	P/07609/005	47 & 49, Stoke Road, Slough, SL2 5AH	Conversion of lower ground floor to create self contained bedsit and flat, new external staircase to rear of the property, loft conversion with rear dormer, 2 no front roof lights and alterations to existing single storey rear extension roof.	NS	2	0	-1	0	1	
Elliman	P/08338/002	LAND R/O, 5-9, ELLIMAN AVENUE	DETAILS OF DETACHED HOUSE SUBMITTED PURSUANT TO CONDITIONS OF PERMISSION REF. P/ 08338 DATED 23.05.89. (AMENDED PLANS DATED 07.01.92)	UC	1	0	0	1	0	
Elliman	P/16652/003	83, Elliman Avenue, Slough, SL2 5AZ	Construction of a 2 storey building with a rear 1 storey to include A1 retail on ground floor. 2bed flat on first floor and studio flat to rear ground floor and rear end of the property to include 6no tandem parking space for the retail unit and flats.	UC	2	0	0	2	0	
Elliman	P/17022/001	6, Shackleton Road, Slough, SL1 3QU	Construction of 1no new dwelling and a first floor rear extension to the existing dwelling	NS	1	0	0	0	1	
Elliman	P/09854/001	1, South Green, Slough, SL1 3QY	Subdivision of the existing two storey side extension to create a 2no bedroom dwelling, together with first floor front/side extension and alterations to include new entrance door. Insertion of new front and side windows and subdivision of existing garden.	NS	2	0	0	0	2	
Farnham	F/00226/040	253-257, Farnham Road, Slough, SL2 1HA	Prior approval for a change of use from office (B1) to residential (C3) at first floor level to provide with 9 flats	UC	9	0	0	9	0	
Farnham	P/00226/043	253-257, Farnham Road, Slough, SL2 1HA	Change of use at ground floor from nursery (D1 Use Class) to provide 3 x self contained residential flats (C3 Use Class) together with integral cycle parking and undercroft parking, external alterations to the facades of the building and erection of single storey extension at roof level above the converted (residential) first floor to provide an additional 6 self-contained residential flats (net increase in 9 x flats excluding the first floor). External cycle store, bin store, railing enclosure, boundary treatment, parking, and	UC	9	0	0	9	0	

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			landscaping. (Revised Description of Development and Revised Plans submitted 10/12/2019 & 08/01/2020)							
Farnham	F/00913/030	Al Quaim Centre, 39, Montrose Avenue, Slough, SL1 4TN	Prior approval notification for a change of use from commercial, business and service (Class E) to residential (class C3) (12no.flats).	NS	12	0	0	0	12	
Farnham	P/00378/029	235, Farnham Road, Slough, SL2 1DE	Change of use of existing top floor storage into 2no flats	NS	2	0	0	0	2	
Farnham	P/00378/031	235, Farnham Road, Slough, SL2 1DE	Outline application with all matters reserved (pursuant to approvals nos: F/00378/027, P/00378/029 and P/00378/030), for the erection of 2 storey plus recessed mansard, side extension to existing front building, over archway, in order to provide a total of one additional flat, (in addition to the 8 approved flats in approvals nos: F/00378/027, P/00378/029 and P/00378/030).	NS	1	0	0	0	1	Outline PP
Farnham	P/00393/009	388-390, Farnham Road, Slough, SL2 1JD	Construction of a first floor extension for 2 x studio flats and the change of use of part of the ground floor from retail to offices (Use Class B1)	NS	2	0	0	0	2	
Farnham	P/00419/017	Iceland Foods Plc, Farnham Avenue, Slough, SL1 4XT	Demolition of existing retail unit (Formerly Iceland Foods Supermarket) and construction of a 4 storey residential building to provide 13no.residential flats (7no; 2 bed; 6 no. 1 bed) units, including 4no. private garages with vehicular crossovers.	UC	13	0	0	13	0	
Farnham	P/00913/032	Al Quaim Centre, 39, Montrose Avenue, Slough, SL1 4TN	Change of use of the ground floor from D1/ B1 to C3 residential use, in order to provide a total of 4 flats, (2 x one-bed. + 2 x 2bed).	NS	4	0	0	0	4	

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Farnham	P/03668/005	Land Rear Of 524 - 526, Farnham Road, Slough, SL2 1HX	Construction of 2no 3 bedroom linked detached dwellings including garage and associated parking and access via existing access	UC	2	0	0	2	0	
Farnham	P/03717/007	240a, Farnham Road, Slough, SL1 4XE	Conversion of 1 four-bed flat into 2 one-bed flats	NS	2	0	-1	0	2	
Farnham	P/12654/009	80, Faraday Road, Slough, SL2 1RS	Retrospective application for the demolition of the existing 2 storey dwelling and construction of a 2 storey dwelling to create 2no flats (amended development to planning permission ref. P/12654/004)	UC	2	0	-1	2	0	
Farnham	P/19425/002	12, Westfield Road, Slough, SL2 1HE	Reserved Matters application pursuant to outline planning permission P/19425/000 dated 03/08/2021 to consider appearance and landscaping, in the respect of the construction of 1no 2 bedroom dwelling	NS	1	0	0	0	1	
Farnham	P/00913/033	Al Quaim Centre, 39, Montrose Avenue, Slough, SL1 4TN	Outline application with all matters reserved for the conversion and extension of existing roof top accommodation to provide 6no flats.	NS	6	0	0	0	6	Outline PP
Farnham	P/08256/002	2, Willoners, Slough, SL2 1SY	Construction of an end terrace 2 bedroom dwelling with associated parking and landscaping	NS	1	0	0	0	1	
Foxborough	P/00331/004	Austin Brothers, 413, London Road, Slough, SL3 8PS	Construction of 14no. flats comprising 13no. 2 bedroom flats and 1no. studio flat with associated parking and amenity.	NS	14	0	-1	0	14	

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Foxborough	P/07230/001	24, Parlaunt Road, Slough, SL3 8BB	Construction of a two storey extension to create 1no 2 bedroom dwelling	NS	1	0	0	0	1	
Foxborough	P/17517/000	Land adj to Quantock Close, Slough, SL3 8UD	Demolition and redevelopment of existing garage site to provide 8no. 2-bedroom (4 person) affordable residential units, with associated car parking, cycle parking, refuse store and landscaping.	NS	8	0	0	0	8	
Haymill & Lynch Hill	P/00442/017	426-430, Bath Road, Slough, SL1 6BB	Reserved matters application for approval of full details regarding matters of Appearance, Landscaping and Layout pursuant to outline planning permission (and Approval of Scale and Access) for demolition of existing buildings and redevelopment to provide up to 75 dwellings, including access, parking, amenity space, landscaping, boundary treatments and associated infrastructure (Ref. P/00442/016) dated 25th September 2020.	UC	75	0	0	75	0	
Haymill & Lynch Hill	P/00838/007	61, Burnham Lane, Slough, SL1 6JX	Construction of a 1no. four bedroom dwelling following demolition of existing.	UC	1	0	-1	1	0	
Langley Kedermister	P/00605/015	Langley Road Dental Practice, 162, Langley Road, Slough, SL3 7TG	Change of use of ground floor flat at rear to Class E(e) Dental Practice, and the construction of a first floor extension and roof to facilitate additional floorspace for the Dental Practice and 1 bed self-contained flat, and the alteration of the ground floor rear extension roof from a lean-to roof to a flat roof single storey extension (amended description)	NS	1	0	0	0	1	Overlap. Subsequent PP
Langley Kedermister	P/00605/016	Langley Road Dental Practice, 162, Langley Road, Slough, SL3 7TG	Change of use of ground floor flat at rear to Class E(e) Dental Practice, and the construction of a first floor extension and roof to facilitate additional floorspace for the Dental Practice and 1 bed self-contained flat, and the alteration of the ground floor rear extension roof from a lean-to roof to a flat roof single storey extension	NS	1	0	0	0	1	

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Langley Kedermister	P/07291/003	155, High Street, Langley, Slough, SL3 8LP	Extension to existing garage and conversion of existing house into 1 x 3 bedroom house and 1 x 1 bedroom house with a new dropped kerb and removal of front garden wall and associated works	NS	2	0	0	0	2	
Langley Kedermister	P/18064/003	105, Langley Road, Slough, SL3 7DY	Construction of a 1no 4 bedroom dwelling following the demolition of existing dwelling and associated works	NS	1	0	0	0	1	
Langley Kedermister	P/02500/009	5, Langley Broom, Slough, SL3 8NB	Demolition of existing bungalow and construction of 2no. 3 bedroom detached dwellings and associated works.	UC	2	0	-1	2	0	
Langley Kedermister	P/03211/004	9, Reddington Drive, Slough, SL3 7QX	Erection of two storey side extension and a single storey rear extension to create a 2no bedroom dwelling with associated car parking and access	NS	1	0	0	0	1	
Langley Kedermister	P/04252/003	104, Blandford Road North, Slough, SL3 7TA	Demolition of existing dwelling and construction of a building entailing a bungalow with accommodation at roof level comprising 2no new dwellings.	NS	2	0	-1	0	2	Overlap. Subsequent PP
Langley Kedermister	P/04252/007	104, Blandford Road North, Slough, Slough, SL3 7TA	Demolition of existing dwelling and construction of a 1no new 4 bedroom dwelling	UC	1	0	-1	1	0	
Langley Kedermister	P/07749/012	172, Langley Road, Slough, SL3 7EE	Demolition of existing dwelling and construction of 2no. semi-detached four bedroom houses	NS	2	0	-1	0	2	



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Langley Kedermister	P/08979/002	Langley Police Station, High Street, Langley, SL3 8MF	Demolition of the existing garages, alterations to the existing entrance / egress from Trelawney Avenue, and redevelopment of the existing site to include - conversion of the former police station (sui generis) to residential accommodation (10 x studio units), construction of 2 x 3 bedroom and 1 x 2 bedroom family units and a 1 x 6 bedroom HMO unit with associated car parking, cycle parking, refuse store and landscaping.	NS	14	0	0	0	14	
Langley Kedermister	P/09718/005	Land R/O, 58, High Street, Langley, Slough, SL3 8LP	Construction of a bungalow at the rear of the garden of 58 High Street.	NS	1	0	0	0	1	
Langley Kedermister	P/19697/000	64, Churchill Road, Slough, SL3 7RB	Construction of an attached 1no 2 bedroom dwelling with pitched roof and parking to the front following the demolition of existing store	NS	1	0	0	0	1	
Langley Kedermister	S/00744/000	Land Adj To Broom House, Langley, SL3 7QZ	Construction of 4no 4 bedroom residential units with associated parking, cycle parking, refuse stores and landscaping.	NS	4	0	0	0	4	
Langley St. Marys	P/02745/004	7, Mina Avenue, Slough, SL3 7BY	Demolition of existing dwelling and garage store and construction of 1no new 4 bedroom dwelling with associate landscaping	NS	1	0	-1	0	1	
Langley St. Marys	P/16337/007	9, Mina Avenue, Slough, SL3 7BY	Construction of two semi-detached dwellings incorporating landscaping, parking p	NS	2	0	-1	0	2	Overlap. Subsequent PP
Langley St. Marys	P/16337/009	9, Mina Avenue, Slough, SL3 7BY	Construction of a detached dwelling incorporating landscaping, parking provision	NS	1	0	-1	0	1	Overlap. Subsequent PP

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Langley St. Marys	P/16337/011	9, Mina Avenue, Slough, SL3 7BY	Construction of a pair of semi-detached dwellings with outbuildings to the rear. The development incorporates landscaping, parking provision and associated development	NS	2	0	-1	0	1	
Langley St. Marys	P/03514/008	274a, High Street, Slough, Langley, SL3 8HD	Conversion of existing commercial storage and warehousing unit (Class E) into a one bedroom accommodation (C3), including partial demolition of shop floor, inclusion of 8no. new windows, removal of rear access door and associated internal and external works	NS	1	0	0	0	1	
Langley St. Marys	P/00679/005	140, Langley Road, Slough, SL3 7TG	Demolition of existing single, 4-bedroom dwelling (use class C3) and erection of 2 semi-detached, 5-bedroom dwellings (use class C3) with associated private amenity space and 3no. off street parking spaces each.	NS	2	0	-1	0	2	Overlap. Subsequent PP
Langley St. Marys	P/00679/010	140, Langley Road, Slough, SL3 7TG	Demolition of existing 4-bedroom dwelling (use class C3) and construction of 2 semi-detached, 5-bedroom dwellings (use class C3) with associated private amenity space and 3no. off street parking spaces each.	NS	2	0	-1	0	2	
Langley St. Marys	P/03099/004	18, Willoughby Road, Slough, SL3 8JH	Demolition of existing porch and construction of a single storey rear extension and remodel roof form in conjunction with conversion of loft space into habitable room with front and rear dormers and construction of 1no new two-storey, three-bedroom dwelling to the side of no 18 Willoughby Road.	NS	2	0	0	0	2	
Langley St. Marys	P/15790/002	21, The Drive, Slough, SL3 7DB	Conversion and extension to existing dwelling to create 2no 3 storey, 2 bedroom dwelling houses.	NS	2	0	-1	0	2	
Langley St. Marys	P/19680/000	94, Meadfield Road, Slough, SL3 8HR	Construction of 2no 3 bedroom semi detached dwellings following demolition of existing dwellinghouse (amended description).	NS	2	0	-1	0	2	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Langley St. Mary's	P/00437/093	Langley Business Centre, Station Road, Slough, SL3 8DS	Outline planning permission with all matters reserved for details of access, appearance, landscaping, layout and scale reserved for later determination. Demolition and redevelopment to comprise on plot (B) a data centre of up to 96,000 sqm gross, including ancillary offices and sub station; and plot (A) up to 9,650 sqm GEA to comprise one or more land uses comprising: up to 60 dwellings (Use Class C3); up to 6,000 sqm gross of B1c (offices); additional development in Use Classes: A1, A2, A3 (retail), A4 (public house), A5 (take away); D1 and D2 (community and leisure) and an energy centre. Development in plot (A) or plot (B) or both may also include: car parking; provision of new plant; creation of servicing areas and provision of associated services, including waste, refuse, cycle storage, and lighting; and for the laying out of the buildings; routes and open spaces within the development; and all associated works and operations including but not limited to: demolition; earthworks; provision of attenuation infrastructure, engineering operations. Development in plot (A) and plot (B) shall be in accordance with the approved Development Parameters Schedule and Plans.	NS	60	0	0	0	60	Outline PP
Langley St. Mary's	P/01272/012	The Former Willow Tree, 62, Station Road, Langley, SL3 8BT	Demolition of existing structures and redevelopment of the site for a part single through to a part five storey building to accommodate 41 residential units, with associated parking and amenity provision	NS	41	0	0	0	41	
Langley St. Mary's	P/06953/003	61, Meadfield Road, Slough, SL3 8HR	Construction of a front porch, single storey rear extension, loft conversion with rear dormer window and proposed subdivision of house into 1x 2No. bedroom and 1x 3No. bedroom houses.	UC	2	0	-1	2	0	
Langley St. Mary's	P/15353/003	110, Willoughby Road, Slough, SL3 8JG	Construction of 1no 3 bedroom bungalow	NS	1	0	0	0	1	
Langley St. Mary's	P/18027/000	1, Downs Road, Slough, SL3 7BR	Construction of 2 no. 5 bedroom dwelling houses with associated landscaping and parking provision.	NS	2	0	0	0	2	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
Langley St. Mary's	P/19132/000	89, Meadfield Road, Slough, SL3 8HY	Construction of 2no 3 bedroom semi detached houses with associated car parking	NS	2	0	0	0	2	
Upton	P/00402/012	Abd House, 34, London Road, Slough, SL3 7HH	Change of use of existing bungalow structure into a self-contained unit for elderly parents	NS	1	0	0	0	1	
Upton	P/03023/007	18, Blenheim Road, Slough, SL3 7NJ	Demolition of existing garage and play room. Construction of porch, part two storey, part single storey side and rear extensions to create 1no x three bedroom dwelling with associated parking and amenity space.	UC	1	0	0	1	0	
Upton	P/04147/003	53, Langley Road, Slough, SL3 7AH	Demolition of existing dwelling and construction of 1no. new 6 bedroom dwelling.	UC	1	0	-1	1	0	
Upton	P/06350/002	Gurney House, Upton Road, Slough, SL1 2AE	Construction of a residential development containing 16 terrace houses including	UC	16	0	0	16	0	
Upton	P/08576/004	11, Rambler Lane, Slough, SL3 7RR	Demolition of existing dwelling and construction of a new 5 bedroom dwelling and ancillary facilities	NS	1	0	-1	0	1	
Upton	P/10382/008	Land R/O, 86-88, Dolphin Road, Slough, SL1 1TA	Demolition of existing houses and construction of 2no. 3 bed houses to the front (semi detached) and three storey terrace of 3 no. 3 bed dwellings to the rear. Formation of vehicular access from Dolphin Road and associated parking.	UC	5	0	-2	3	2	
Upton	P/12308/005	17, Palmerston Avenue, Slough, SL3 7PU	Construction of 1no new detached dwelling including 3no bedrooms, loft space and dropped kerb, 3no car parking spaces & main entrance access.	NS	1	0	0	0	1	
Upton	P/16947/002	45, The Myrke, Datchet, Slough, SL3 9AB	Construction of 2 no. semi-detached dwellings and 1 no. detached dwelling.	UC	3	0	0	3	0	
Upton	P/16972/002	Land adjoining, 6, Drake Avenue,	Construction of a new three bedroomed dwelling and associated works, following the relocation of the existing sub station and relocation of first floor side window to front elevation at No. 6 Drake Avenue.	NS	1	0	0	0	1	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
		Slough, SL3 7JR								
Upton	P/18034/002	Land Adj. 19, Glenavon Gardens, Slough, Slough, SL3 7HW	Construction of a 3 bedroom detached house	UC	1	0	0	1	0	
Upton	P/01536/009	55 Alpha Street, South &, 34-36, Hencroft Street, Slough, Berks, SL1 1RD	Redevelopment to provide one pair of two bedroom semi detached dwellings and one pair of three bedroom semi detached dwellings with pitched roofs	NS	4	0	0	0	4	
Upton	P/04949/012	12, London Road, Slough, SL3 7HG	Demolition of existing building and structures and replacement with three dwellings, car parking, landscaping and associated ancillary development	NS	3	0	-1	0	3	
Wexham Lea	P/09115/006	38, Knolton Way, Slough, SL2 5TJ	Construction of 1no. two bedroom dwelling, new access and associated works.	UC	1	0	0	1	0	
Wexham Lea	P/16915/001	39, Berryfield, Slough, SL2 5SA	Demolition of existing garage. Construction of one new dwelling with new vehicular access, parking and rear garden. Single storey rear extension to the existing house.	NS	1	0	0	0	1	
Wexham Lea	P/17921/001	114, Hazlemere Road, Slough, SL2 5PW	Construction of 1no new attached dwelling	NS	1	0	0	0	1	
Wexham Lea	P/19200/000	290, Rochfords Gardens, Slough, SL2 5XW	Construction of 1no two bedroom end terrace dwelling with associated on site car parking.	UC	1	0	0	1	0	
Wexham Lea	P/14878/010	54, Farm Crescent, Slough, SL2 5TH	Construction of a front extension and a two storey side extension to accommodate an attached 1no two bedroom dwelling	UC	1	0	0	1	0	
								1121	2668	

ward	reference	address	development description	construction status (site)	total gross units for site	gross completed	net completed (or figure to reflect existing/future loss)	under construction (UC)	not started (NS)	exclude from 5 year land supply calculation
			less overlapping planning permissions re 'not started' (9).						-9	
			Re 5 year land supply calculation exclude (i) outline PP (1132) (ii) development most unlikely to progress (6) (iii) losses (minus figures in 'net completed' column (ignoring overlaps) (35)  5 year land supply calculation input : 1121 uc + 2659 ns - 1138 outline pp or not progress - 35 expected losses = 2607 relevant homes (net) with consent and not completed.						2659	

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**Table C Core Strategy Plan Period to 2023 residential net completions compared to target/need.**

<b>Year</b>	<b>Past completions Net</b>	<b>Completions Cumulative</b>	<b>Local Housing Requirement/Need (annualised)</b>
2006/07	409	409	315
2007/08	849	1,258	315
2008/09	595	1,853	315
2009/10	275	2128	315
2010/11	249	2,377	315
2011/12	246	2623	315
2012/13	182	2805	315
2013/14	396	3201	315
2014/15	507	3708	315
2015/16	789	4497	550
2016/17	521	5018	550
2017/18	846	5864	550
2018/19	534	6398	893
2019/20	503	6901	893
2020/21	501	7402	863
2021/22	532	7934	864
2022/23	325	8259	847
			8845

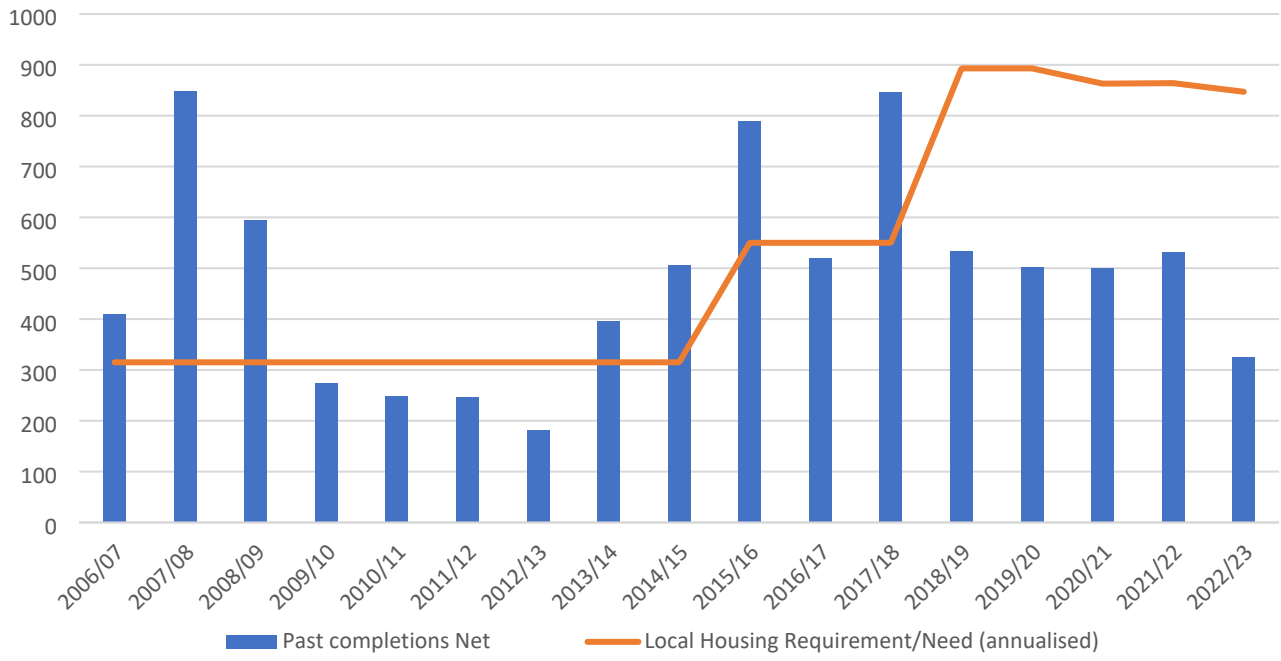
**Local Housing Requirement/Need figure explanation**

315 Core Strategy Housing Target

550 Housing Target self imposed by Council 2015

2018/19 onwards Local Housing Need (re proposed national Housing Methodology confirmed Dec 2020)

Past net completions compared to housing target/need



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**Table D Housing trajectory new Local Plan period**

Local Plan Year	Financial Year	Net Additional Homes	Net Additional Homes Projected	Cumulative	Shortfall against LHN	Local Housing Need (LHN)
			See note below			annual average homes
1	20/21	501		501	362	863
2	21/22	532		1033	332	864
3	22/23	322		1355	525	847
4	23/24		559			825
5	24/25		518			825
6	25/26		476			825
7	26/27		703			825
8	27/28		910			825
9	28/29		1040			825
10	29/30		921			825
11	30/31		722			825
12	31/32		511			825
13	32/33		439			825
14	33/34		437			825
15	34/35		424			825
16	35/36		383			825
17	36/37		369			825
18	37/38		355			825
19	38/39		329			825
20	39/40		212			825
21	40/41		152			825
totals		1355	9461		1219	17424

**Note re projected homes figures**

Net additional homes projected figure of 9,641 calculated from a list of major housing sites (Akzo Nobel site excluded) discounted as follows; an assumed lapse/non implementation rate of 10% over first 5 years and 20% for remainder of plan period. And 60/year small sites estimate added.

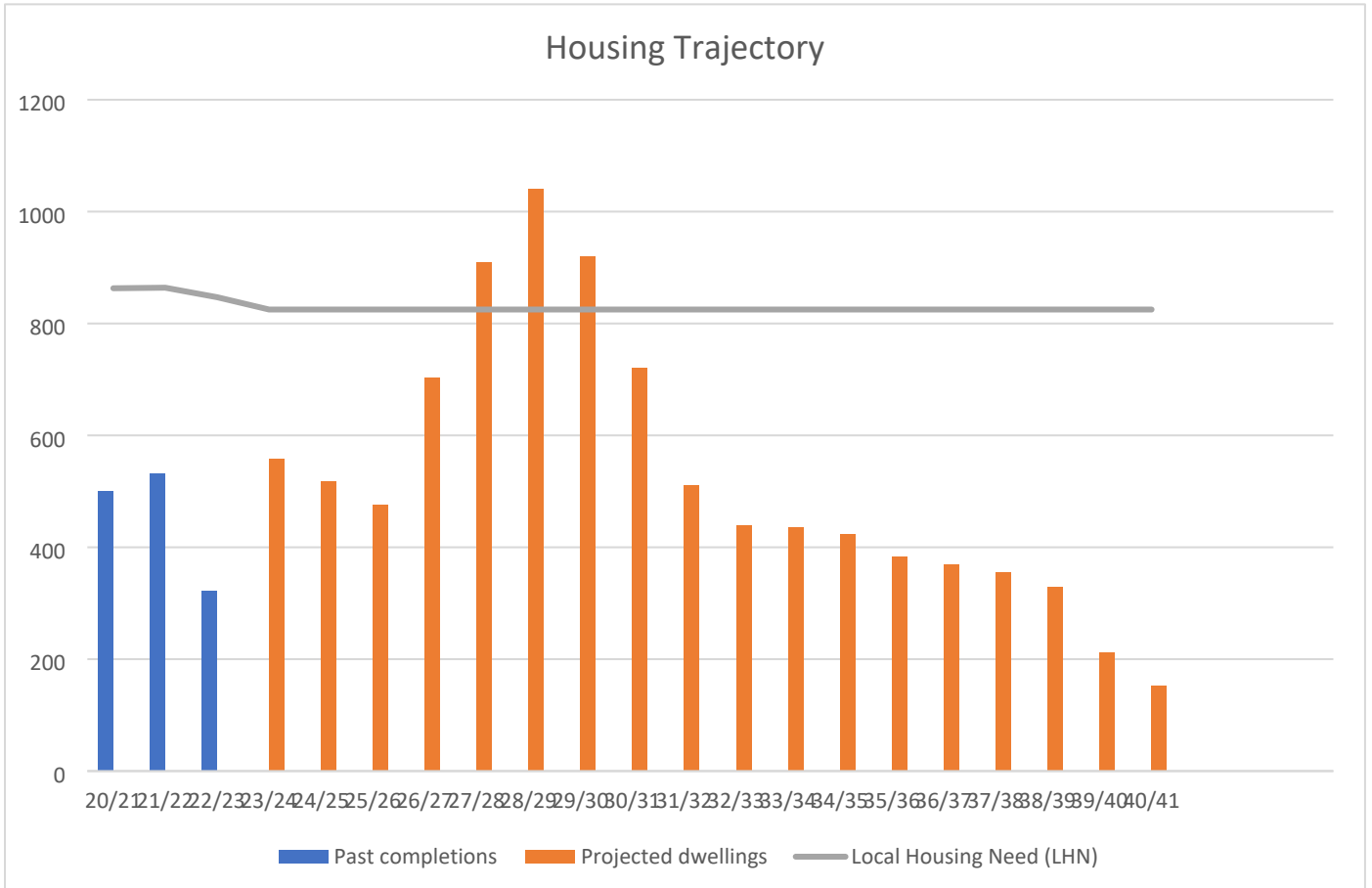
Annual projected figure is rolling average calculated over a 3 year period inclusive of year before and after. Trajectory sites and phasing to be reviewed/refined when further information received from developers/owners

**Note re Local Housing Need**

As at 1/4/23 LHN minus Net Additional Homes=LHN remaining

17,424 – 1,355 = 16,069 LHN for remainder of plan period (18 years) after 1st April 2023. Equivalent to 893/year average.

LHN for remainder of plan period minus projected net additional homes equals likely shortfall. 16,069 - 9,461 = 6,608 shortfall.



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**SLOUGH BOROUGH COUNCIL**

REPORT TO: PLANNING COMMITTEE

DATE: October 2023

**PART 1****FOR INFORMATION****Planning Appeal Decisions**

*Set out below are summaries of the appeal decisions received recently from the Planning Inspectorate on appeals against the Council's decisions. Copies of the full decision letters are available from the Members Support Section on request. These decisions are also monitored in the Quarterly Performance Report and Annual Review.*

**WARD(S)****ALL**

<b>Ref</b>	<b>Appeal</b>	<b>Decision</b>
APP/J0350/C/22/3303574	23, Kent Avenue, Slough, SL1 3AB  Without planning permission the change of use of an outbuilding to form a self-contained dwelling and facilitating works at Land	Appeal Dismissed with Variations  6 <sup>th</sup> November 2023



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## Appeal Decision

Site visit made on 29 August 2023

**by Stuart Willis BA Hons MSc PGCE MRTPI**

an Inspector appointed by the Secretary of State

Decision date 06 November 2023

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**Appeal Ref: APP/J0350/C/22/3303574**

**Land at 23 Kent Avenue, Slough SL1 3AB**

- The appeal is made under section 174 of the Town and Country Planning Act 1990 (the Act) as amended. The appeal is made by Mr Ahsan Ulah against an enforcement notice issued by Slough Borough Council.
- The notice was issued on 14 July 2022.
- The breach of planning control as alleged in the notice is without planning permission the change of use of an outbuilding to form a self-contained dwelling and facilitating works at Land in the approximate location shown edged blue on the Plan.
- The requirements of the notice are:
  1. Cease the use of the outbuilding as self-contained unit of residential accommodation.
  2. Remove the kitchen and shower room from the outbuilding.
  3. Remove the internal walls incorporating the shower room.
  4. Remove all plumbing and associated pipework in connection to the kitchen and shower room within the outbuilding.
  5. Remove from the land all materials, rubbish, debris, plant and machinery resulting from compliance with the above requirements.
- The period for compliance with the requirements is: 5 calendar months.
- The appeal is proceeding on the grounds set out in section 174(2)(a), (b) and (f) of the Town and Country Planning Act 1990 as amended. Since an appeal has been brought on ground (a), an application for planning permission is deemed to have been made under section 177(5) of the Act.

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**Summary of Decision: The appeal is dismissed, and the enforcement notice is upheld with variations in the terms set out below in the Formal Decision.**

### Preliminary Matters

1. The appeal form did not indicate the appeal was sought under ground (b), that those matters stated in the notice have not occurred. However, the appellant's evidence put forward the case that the building was not in use solely as a self contained dwelling.
2. The appeal form did also not indicate that the appeal was sought under ground (f), that the steps required by the notice to be taken, or the activities required by the notice to cease, exceed what is necessary to remedy any breach of planning control which may be constituted by those matters or, as the case may be, to remedy any injury to amenity which has been caused by any such breach. Nevertheless, the appellant raises concerns over the removal of the kitchen.
3. The Council has had opportunity to comment on the evidence relevant to these grounds of appeal, and I am therefore satisfied that no injustice would be caused by considering the appeal under ground (b) and (f).

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<https://www.gov.uk/planning-inspectorate>

### **The Enforcement Notice**

4. During the course of the appeal, planning permission was granted at the appeal building<sup>1</sup> described as 'part retrospective application for a rear outbuilding for use as a bedroom/gym'. The Council subsequently varied the notice under Section 173A(1)(b) of the 1990 Act. The relevant parties were notified, and the appellant had opportunity to comment on the changes and therefore there has been no injustice caused by this. It varied the notice by deleting 'and the shower room' from paragraph 5. 2), all of paragraph 5. 3) 'remove the internal walls incorporating the shower room', and the words 'and shower room from paragraph 5. 4) of the notice.
5. Paragraph 3. of the notice referred to a 'self contained dwelling', whereas the requirement under paragraph 5. 1) stated 'self contained unit of residential accommodation'. In the interest of clarity and precision, I shall delete 'unit of residential accommodation' from paragraph 5. 1) and insert 'dwelling'.
6. I am satisfied that the variation will not cause injustice to the appellant or the local planning authority.

### **Reasons**

#### **The appeal on ground (b)**

7. The appellant indicates that the building subject to the notice was partly in use as an office associated with 23 Kent Avenue as well as being used as a separate dwelling. Whatever the original purpose of the building, there is no clear evidence before me that there was any office use at the time of the notice being issued. Moreover, at the time of my site visit there was no apparent office element. Therefore, those matters stated in the notice have occurred and the appeal fails on ground (b).

#### **The appeal on ground (a) and the deemed application**

8. The appeal on ground (a) is that, in respect of any breach of planning control which may be constituted by the matters stated in the notice, planning permission ought to be granted. With a ground (a) it is incumbent on me to consider whether planning permission should be granted for the whole or part of the matters stated in the notice in accordance with Section 177(1) of the Act.

### **Main Issues**

9. The main issues are:
  - the effect of the development on the character and appearance of the area,
  - whether the development provides appropriate living conditions for occupiers of the appeal building with particular regard to outlook, internal and external space and privacy,
  - the effect of the development on the living conditions of nearby occupiers with particular regard to privacy, noise and disturbance; and
  - the effect of the development on highway safety, with particular regard to car parking and cycle storage facilities.

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<sup>1</sup> P/19907/000

## Reasons

### *Character and appearance*

10. Kent Avenue and the surrounding streets are characterised by linear frontage properties that address the street with their main garden space to the rear. This gives a pleasing consistency to the pattern of development and spaciousness between properties. While there are outbuildings of a comparable size in the street, no examples of any dwellings located behind the frontage properties nearby and with no outdoor space, like the appeal scheme, have been put to me. The appeal scheme unfavourably contrasts with, and erodes, the prevailing pattern of development in the area having led to a cramped appearance and discordant layout.
11. Therefore, the development has significantly harmed the character and appearance of the area. It is contrary to Policies EN1, H13 of The Local Plan for Slough (Local Plan) and Saved Core Policies 1, 4 and 8 of the Slough Local Development Framework Core Strategy (Core Strategy). These in part seek developments to be compatible, in keeping with, and related to their surroundings and prevent over development.

### *Living conditions - Occupiers of the appeal building*

12. There is limited internal space at the appeal building and no outdoor space associated with it. This has led to a cramped and uncomfortable living environment for the occupiers.
13. The only windows in the building face 23 Kent Avenue (No 23) which is only a short distance away. As a result, No 23 dominates the outlook from the windows giving an uninviting and oppressive outlook from them.
14. As the garden of No 23 extends up to the appeal building, there are direct close range views into the rooms of the building meaning insufficient privacy is provided for occupiers of it.
15. Consequently, the development significantly fails to provide appropriate living conditions for occupiers of the appeal building with regard to outlook, internal and external space and privacy. It is contrary to Policies H13, H14 of the Local Plan and Core Policy 8 of the Core Strategy where they require appropriate and quality amenity space, as well as seek to prevent loss of privacy.

### *Living conditions – nearby occupiers*

16. At my visit, while only a snapshot in time, I found the rear garden areas at and near the site to be relatively private and quiet spaces. To access the appeal building occupiers or visitors have to pass close to the side of No 23 and then through its garden. The comings and goings associated from a separate dwelling is untypical in this position and creates unacceptable noise and disturbance to the rear of No 23.
17. Given the presence of boundary treatments and that the only openings are on the front elevation of the appeal building, noise or disturbance from movements associated with it would not be as noticeable or unacceptable at the adjacent and other nearby properties.
18. There is no separation between the windows at the appeal building and the garden of No 23. In addition, windows at No 23 are in close proximity to those



in the appeal building. Consequently, the direct close range views from the appeal building significantly reduce the privacy for occupiers of No 23.

19. Boundary treatments and the position of the openings at the appeal building prevent any overlooking into the ground floor windows and gardens of the other nearby properties. Views towards upper floor windows would be angled and outbuildings in a similar position are not untypical. Therefore, views from such a position would not be unexpected or direct, thus preventing unacceptable effects on privacy at other nearby properties.
20. Nevertheless, the development significantly harms the living conditions of the occupiers of No 23 with regard to privacy, noise and disturbance. The development is contrary to Policy H13 of the Local Plan, where it seeks to prevent loss of amenity and privacy at existing properties.

#### *Highway Safety*

21. Occupiers of the appeal building may try to park on the drive of No 23. At my visit, I saw a vehicle parked on the drive overhanging the pavement indicating insufficient space to accommodate the requirements of the occupiers of both No 23 and the appeal building. As such, it is likely they would park on the street.
22. Given my findings regarding internal space for occupiers and the lack of any outdoor space, there would be no provision for bicycles. Therefore, the appeal scheme does not encourage the use of alternative modes of transport. This increases the likelihood of using private cars and the likelihood of vehicles needing to be parked on the street.
23. I saw many properties in the street have driveways, limiting on street parking spaces. Even so I saw several instances vehicles parked on driveways overhanging the pavement. While my visit was early afternoon, there was considerable on street parking taking place with the narrowness of the road meaning on street parking frequently involved vehicles parking partly on the pavement and near to junctions.
24. Additional vehicles associated with the appeal building add to the parking congestion. In light of the limited on-street spaces available, the appeal scheme is likely to lead to instances of dangerous and obstructive parking such as at road junctions, on footways or across driveways. This would add to existing highway congestion and as a result further compromise the safety of pedestrians and road users.
25. As such, the development has caused moderate harm to highway safety with particular regard to car parking and cycle storage facilities. It fails to accord with Policies H13 and H14 of the Local Plan and Core Policies 7 and 8 of the Core Strategy where they require appropriate access, improving road safety and be safe with sufficient parking provision while widening travel choices.

### **Other Matters**

26. While the height of the building may be within permitted development limits for a building incidental to the enjoyment of a dwellinghouse<sup>2</sup>, the appeal relates to a self contained dwelling.

### **Planning Balance**

27. Given the scale of the scheme, the contribution towards housing supply and economic benefits associated with the development attract small weight. The evidence indicates that personal issues have led to the current occupant living at the appeal building. However, on the limited information before me in this regard and about any alternatives available to them, I give this moderate weight.
28. The development gives rise to significant harm to the character and appearance of the area, the living conditions of the occupiers of No 23, and fails to provide appropriate living conditions for occupiers of the building. There is also moderate harm to highway safety.
29. The policies the development conflicts with are consistent with the Framework and therefore I afford the conflict with them substantial weight. The proposal conflicts with the development plan as a whole.
30. The Council acknowledge they are unable to demonstrate a 5-year supply of housing land meaning that paragraph 11d) of the National Planning Policy Framework (Framework) applies. The scheme aligns with the aims of the Framework to significantly boost the supply of housing and help build a strong economy. As it relates to 1 dwelling, this attracts small weight.
31. The development is contrary to the Framework where it aims to ensure developments are sympathetic to local character, there is a high standard of amenity for existing and future users, development should be prevented on highways grounds if there would be an unacceptable impact on highway safety and where it seeks to ensure appropriate opportunities to promote sustainable transport modes have been taken up. This harm, and weight attracted by it, would be substantial.
32. Having considered the above factors, the adverse impacts of the development would significantly and demonstrably outweigh the benefits when assessed against the policies in the Framework as a whole.

### **Conclusion on ground (a) and the deemed planning application**

33. The development is contrary to the development plan as a whole and there are no material considerations of sufficient weight to indicate that the appeal should be determined other than in accordance with the development plan.
34. I conclude that planning permission should not be granted for the matters stated in the notice and the appeal fails on ground (a).

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<sup>2</sup> Under Class E, Part 1, Schedule 2 of the Town and Country Planning (General Permitted Development) (England) Order 2015



### **The appeal on ground (f)**

35. Since the notice requires the cessation of the use and the removal of facilitating works, the purpose of the notice is to remedy the breach. The evidence before me indicates the kitchen is integral and solely for the purpose of facilitating the unauthorised use so its removal is not excessive.
36. After the issuing of the notice, planning permission was granted for the building to be used as a bedroom/gym, which included the retention of the shower room. Section 180 of the Act states that where after the service of a copy of an enforcement notice planning permission is granted for any development carried out before the grant of that permission, the notice shall cease to have effect so far as inconsistent with that permission.
37. Therefore, the planning permission overrides the notice to the extent that the permission authorises what is being enforced against. As such, the requirements involving the removal of the shower room, its walls and pipework are overridden by the planning permission. All other parts of the notice and the requirements to comply with it still have effect.
38. I have varied the requirements of the notice, with regard to the amended notice. The appeal succeeds to that extent on ground (f) as a result.

### **Conclusion**

39. For the reasons given above, I conclude that the appeal should not succeed. I shall uphold the enforcement notice with variations and refuse to grant planning permission on the application deemed to have been made under section 177(5) of the 1990 Act as amended.

### **Formal Decision**

40. It is directed that the enforcement notice is varied by:
- deleting 'unit of residential accommodation' from paragraph 5. 1) and inserting 'dwelling'.
  - deleting 'and the shower room' from paragraph 5. 2),
  - deleting all of paragraph 5. 3) 'remove the internal walls incorporating the shower room'; and
  - deleting 'and shower room' from paragraph 5. 4).
41. Subject to the variations, the appeal is dismissed, the enforcement notice is upheld and planning permission is refused on the application deemed to have been made under section 177(5) of the 1990 Act as amended.

*Stuart Willis*

INSPECTOR

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**MEMBERS' ATTENDANCE RECORD 2023/24**  
**PLANNING COMMITTEE**

COUNCILLOR	31/05 cancel led	28/06	26/07	06/09 extraor dinary	27/09	25/10 cancel led	29/11	20/12	31/01	28/02	27/03	24/04
Iftakhar		P	P	P	P							
Carter		Ap	P	P	P							
Mann		P	P	P	P							
Gahir		P	P	P	P							
Khawar		P	P	P	Ap							
Naveed		Ap	P	P	P							
Satti		Ap	P	P	P							
Stedmond				P	Ap							

P = Present for whole meeting  
Ap = Apologies given

P\* = Present for part of meeting  
Ab = Absent, no apologies given

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